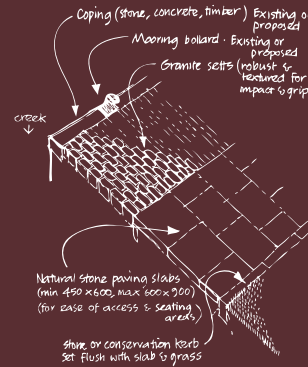
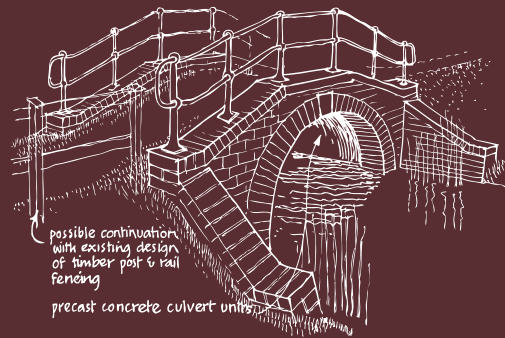
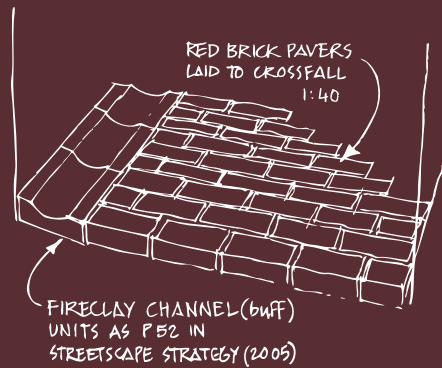




# Faversham Creek Streetscape Strategy



FAVERSHAM CREEK STREETSCAPE STRATEGY

Commissioned by  
Faversham Enterprise Partnership on behalf of Faversham Town Council

Sponsored by  
The Bensted's Charity, Faversham Municipal Charities

Prepared in consultation with  
Faversham Joint Public Realm Working Group, including representatives  
of Swale Borough Council and Kent County Council

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Faversham Creek Streetscape Strategy

## 1.00 Introduction

This document was commissioned by the Faversham Enterprise Partnership on behalf of Faversham Town Council and sponsored by Kent County Council, The Faversham Enterprise Partnership, the Bensted's Charity and the Faversham Municipal Charities, in December 2011. The detailed preparation has been undertaken with the Faversham Joint Public Realm Working Group, which includes representatives of the town council, Swale Borough Council and Kent County Council.

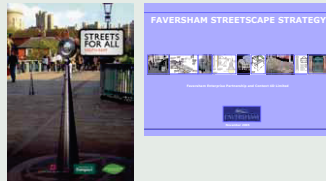
In addition to those partners above, the study has developed through consultation with a number of landowners and businesses within the creekside area. A period of public consultation commenced with an exhibition of the draft proposals held in Faversham on 10th March 2012, with opportunities for responses over subsequent weeks. It has been endorsed by Faversham Town Council, Swale Borough Council and Kent County Council as guidance for future improvements to the public realm at Faversham Creek.

The aim of this study is to develop a strategy for the improvement of the footpaths, streetscape and public realm of Faversham creekside, as shown on (fig 01).

The Strategy is complementary to the Faversham Streetscape Strategy (2005), covering the town centre and adjacent residential streets, adopted by the Joint Transportation Board of Kent County Council and Swale Borough Council. The broad aims and approach adopted in the Faversham Streetscape Strategy are identical to this strategy. However, planning issues concerning the uses, existing or proposed, or the detailed design of development, are not within the remit of this strategy.

Almost all of the creekside area lies within the designated Faversham Conservation Area and therefore the Conservation Area Character Appraisal has also been taken into account in the development of this strategy.

It is intended that the two strategies will be delivered as a single town-wide set of measures through the agreed mechanism of the Faversham Joint Public Realm Working Group.



These documents should be consulted in conjunction with this Strategy.

## 1.00 Introduction. continued

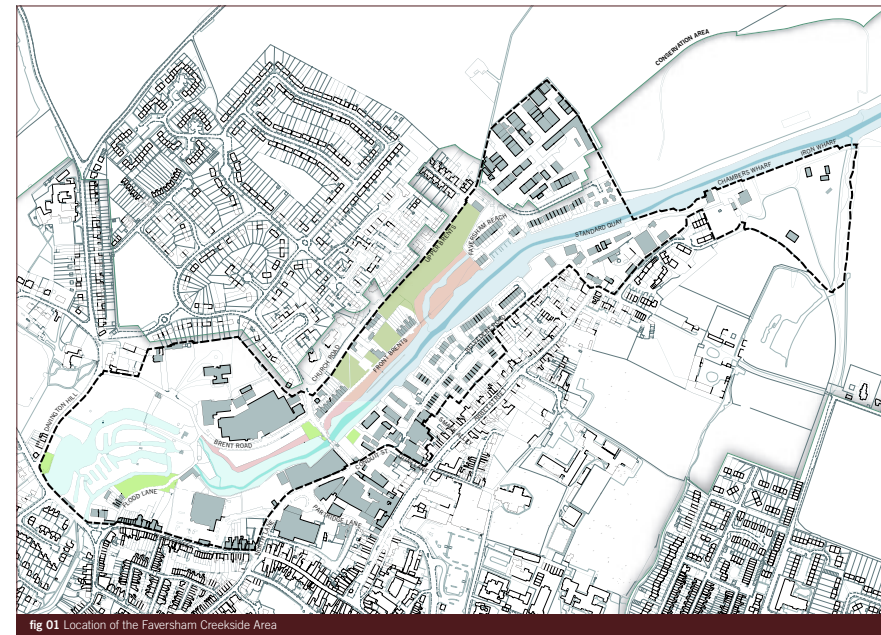


fig 01 Location of the Faversham Creekside Area

## 1.00 Introduction. continued

1.01  
**WHY IS A STREETScape STRATEGY REQUIRED FOR THE CREEKSIDE AREA?**

The Faversham Streetscape Strategy (2005) has successfully influenced the coordinated approach to improvements to street surfaces and street furniture in the town centre and the Tony Fullwood Associates report on Faversham Creek Area Action Plan, Developing Proposals and Future Planning Policy Options to deliver regeneration of the Creek Area (October 2010) proposed that this approach be extended in more detail to the Faversham Creek area as an integral part of its regeneration. This approach is supported by Faversham Town Council, Swale Borough Council, Kent County Council, Faversham United Municipal Charities, the Bensted's Charity and Faversham Enterprise Partnership.

As both the appraisal of the character of the creekside and the appraisal of the issues in the streetscape (sections 2.00 and 3.00) demonstrate, the creekside has a set of unique character issues, distinct from but related to those of the town centre and wider town.

The streetscape or public realm, those spaces between buildings and the creek itself, are the setting for the activities and buildings of the area and is a major contributor to its character.

Streetscape is used in this context to refer to any route and its surroundings, whether urban or rural, informal or formal, hard or soft.

Many interests compete for space in the public realm: pedestrians, residents, visitors and workers, bicycles, buses, cars and heavy goods vehicles, moving or parked. The responsibilities for the public realm are diverse, and therefore decisions affecting it can be uncoordinated, resulting in a sometimes cluttered and uncared-for appearance.

For all these reasons it is recognised that the streetscape as a whole requires a strategy to coordinate design, interventions and maintenance by all those who have responsibilities within the public realm; the local community, the local councils at all levels, utility companies, businesses and developers.

**It is important that future work within the public realm of Faversham Creek follows the guidance in this strategy, in order that the use of scarce resources can be maximised in contributing to its improvement.**

## 1.00 Introduction. continued

1.02  
**THE STRUCTURE OF THIS STRATEGY.**

The principal aim of the strategy is that improvements in the public realm should respond to and enhance the character and distinctiveness of the creekside area.

**Therefore in:****Section 2.00**

The characteristics of the creekside public realm are analysed and appraised, and set within the character of each part of the area.

**Section 3.00**

summarises the issues arising from an analysis of the public realm (condition, legibility, connectivity etc).

**Section 4.00**

sets out the guiding principles of the strategy.

**Section 5.00**

outlines the action plan for the public realm.

**Section 6.00**

identifies specific projects which would individually and as a whole, contribute to the improvement of the public realm.

**Section 7.00**

sets out indicative costings for each project.

**Section 8.00**

provides guidance on implementation of the Strategy.

## 2.00 The Character of Creekside and its Streetscape

The focus and raison d'être of the character of the Creekside area is Faversham Creek itself, as it runs from Stonebridge Pond in the west, through the Basin then past the quays on the town side, and the softer green banks on the Brents side, then turns north to join the Swale.

The creek side's character is formed by these areas, different from one another in terms of topography, sense of enclosure, functions (past and present), built form and urban or rural characteristics.

While Faversham Creek is the area's unifying thread, its changing nature accentuates its diversity; a freshwater stream and pond to the west, a commercially enclosed, tidal basin further east, then a more open aspect through a chiefly urban, predominantly residential locale with green spaces formed by Front Brents, Crab Island, Faversham Reach and Waterside Close to the north and Provender Walk to the south, and then by the more industrialised areas formed by the former oil depot, coach depot, Standard Quay and Iron Wharf, and finally north as a more sinuous, more natural waterway running through farm and marsh land to the Swale.

Because of this uniquely diverse nature it is logical that design and management solutions differ, in response to those differing characters.

**The creekside area has been divided into character sub areas (see map fig 02) to reflect this diversity. These character areas are appraised below and the nature of the streetscape in each is identified.**

## 2.00 The Character of Creekside and its Streetscape. continued

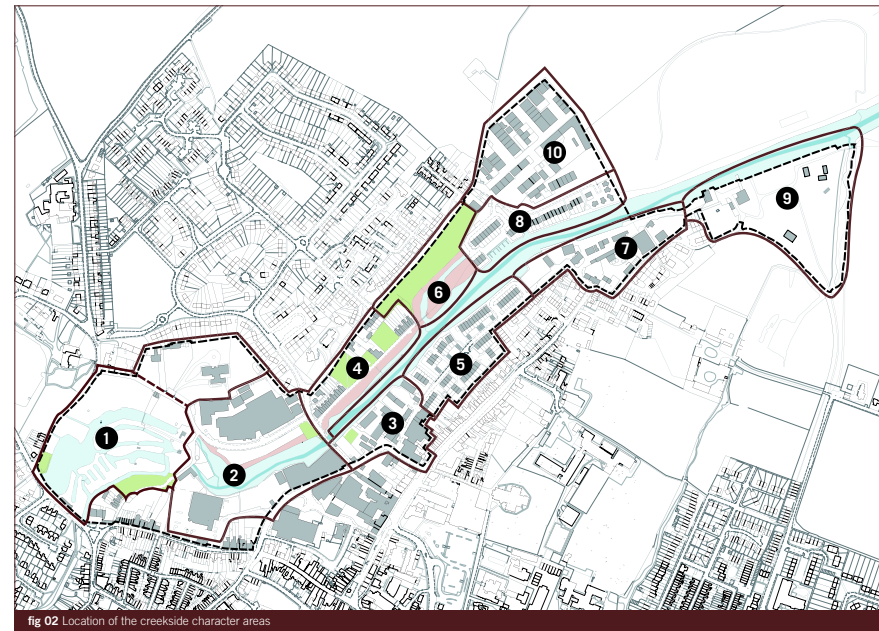


fig 02 Location of the creekside character areas

## 2.01 Stonebridge Pond. Character Area 1

This secluded area, hemmed in by walls and hedges on its northern and western sides, with land rising up Davington Hill is the highest point of the Creekside area. Views across the area to the landmark of the pyramidal roof topping the tower of Davington Church, can be seen from a number of vantage points. Trees form a major enclosing aspect of the area, as do the attractive cranked red brick walls either side of the road up Brent Hill. The man made ponds, interconnected linear stretches of water, can be seen from gaps in wall and hedge and from Flood Lane. This is a somewhat secretive area as it is occupied by well used allotments. One of the few vantage points from which to view the area is a small greenspace to the western end of West Street. This attractive space, equipped with seat and interpretation board is something of a dead end. There is potential to create a link to Davington Hill, thus bypassing a corner with little provision for the pedestrian.

The other view is from the greenspace at the eastern end of Flood Lane. The lane has an informal character greatly valued by residents. The eastern half of Flood Lane is marked out by timber posts to prevent vehicles intruding onto the green space. Its surface is breaking up and has potholes in places. Due to pressure of vehicle parking and the poor surface, walkers are creating a new route across the green. As the water table is very close to the surface it is prone to large puddles.



fig 03

fig 03

The characteristic red brick walls of Brent Hill, opening and closing views. Note the shared surface on this narrow lane.



fig 04

fig 04

A fine boundary wall made of local flints encloses Davington Hill. The pavement is very narrow at this point.

fig 05

Stonebridge Pond at its westernmost end is a more open area. The tower of Davington Church is on the ridge.



fig 05

Faversham Creek Streetscape Strategy

## 2.01 Stonebridge Pond continued.



fig 06

fig 06

A rare general view of the allotments glimpsed from Brent Hill.

fig 07

The extensive allotments glimpsed through an ivy clad gateway near the foot of Davington Hill.

fig 08

Stonebridge Pond from Flood Lane. The water provides a natural boundary between green space and allotments.

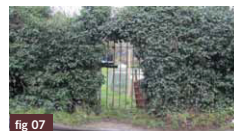


fig 07



fig 08



fig 09



fig 10

fig 09

Flood Lane. Timber posts prevent vehicle encroachment onto the green. The lane surface is breaking up and is bypassed by pedestrians when the potholes are flooded.

fig 10

The restful viewing area, with interpretation board, just at the foot of Davington Hill.

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## 2.02 The Basin. Character Area 2

Here the creek widens to an impounded stretch of water (at high tide) or mud, when the tide is out. It is bounded by the wharfs on its south side and the west (Ordnance Wharf), and the softer, verdant banks on the north side. It is here in the Basin that the larger floorplate buildings are to be found; the factory of BMM Weston fronting Brent Road, the bottling plant of Shepherd Neame, and associated distribution yard, and Morrisons foodstore. Whilst these may not be the most attractive of buildings, they represent the larger scale business premises along the creekside. The Morrisons building has been designed to provide roofscape interest, recalling traditional industrial buildings. The Purifier building, situated at the western end where the creek emerges from a bridge between the it and the Ordnance Wharf, is an attractive, yet long vacant building. Plans for its reuse and conservation are being progressed at the time of writing. Due to its design, location and original use, this building is a heritage asset, directly addressing the Creekside footpath routes. Ordnance Wharf is a peninsular between the two outlets of Stonebridge Pond. At present it is an almost totally cleared site. Its historic brick retaining walls are in poor condition, but its location offers good views along the Basin.

The unmade track running behind both the Purifier building and Ordnance Wharf is in need of repair. The unmade track running behind both the Purifier building and Ordnance Wharf is in need of repair. At present it is chiefly a footpath with minimal vehicular movements for access to the allotments and Ordnance Wharf.

The track emerges at the point where Brent Road, built to conventional highway standards meets the foot of the older Brent Hill, a narrower shared surface lane between high brick walls. The footpath 'disappears' into the carriageway at this point before picking up at the pavement of

Brent Road or the footpath following the northern bank of the Basin. This footpath, recently surfaced with compacted gravel edged in timber, wanders between the bushes and trees along the bank and the BMM Weston car park. It emerges at its eastern end onto a grassed area some way short of Bridge Road and Front Brents.

**fig 13**  
The basin path emerges abruptly from the left onto Brent Road.



**fig 13**

**fig 14**  
Flood Lane narrows to a path on the north side of the Purifier building on the left.

**fig 15**  
The sensitive detailing of the materials and layout of the path on the north side of the Basin.



**fig 14**



**fig 15**

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## 2.02 The Basin. continued

The Shepherd Neame bottling plant and yard occupy a considerable stretch of the southern side of the basin, with premises extending to the wharf wall, making public access impossible. The need for security and the utilitarian nature of the building result in almost 'dead' frontages facing the Basin. The high screen wall for Morrisons yard and the north elevation of the foodstore have a similar effect, although relieved by a quayside walkway along this frontage, surfaced with block pavers and protected by bollards and chains. The walkway turns south between the blank walls of both premises; it is approached at either end by short flights of steps. The existence or

destination of this path is unclear until it is reached. Its appearance between two walls topped by security wire is extremely unattractive. The combination of these factors means that it is hardly used and has a reputation as being unsafe. The views from the 'Morrisons Wharf' area both to the west and east, are attractive at various states of the tide and the day.



**fig 16**



**fig 17**



**fig 18**



**fig 19**

**fig 16**  
The junction with Flood Lane on the left, where Brent Road narrows to the enclosing walls of Brent Hill.

**fig 19**  
Flood Lane looking towards Ordnance Wharf.

**fig 17**  
The Basin path seen from the bridge, across the green space.

**fig 20**  
The quay on the north side of Morrisons with the Purifier building on the left and Ordnance Wharf ahead.

**fig 18**  
The informal path along the north side of the Basin, looking towards the bridge.



**fig 20**

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2.02 The Basin. continued



fig 21

**fig 21**  
The Basin looking east from the quay on the north side of Morrisons store.

**fig 22**  
The southern end of the path between the walls of Morrisons and Shepherd Neame's delivery areas.

**fig 23**  
The apparent dead end of the path on the north side of Morrisons yard continues unpromisingly to the right.

**fig 24**  
The path diverts south between the yards of Shepherd Neame's and Morrisons premises.



fig 22



fig 23

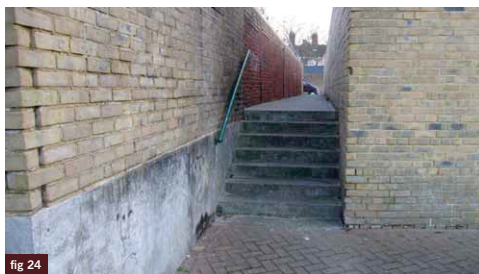


fig 24

2.02 The Basin. continued

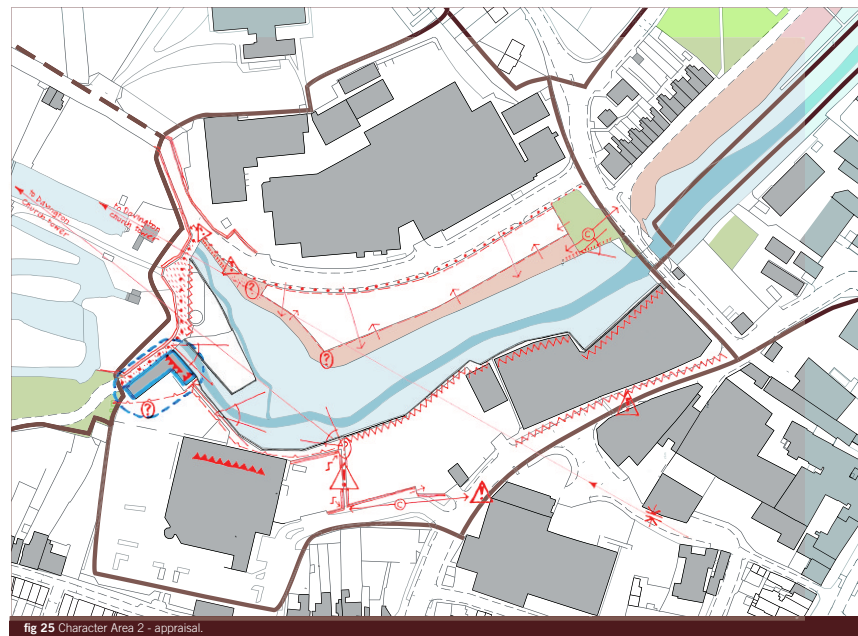


fig 25 Character Area 2 - appraisal.

## 2.03 The Town Quay, Quay Lane and 'Square'. Character Area 3.

This is one of the key focal points of the creekside area; where the town centre and the quayside are most directly connected both visually; in terms of historic buildings and materials, and physically, in terms of a direct footpath link. The direct sight line between Court Street via Quay Lane to the waterfront and Front Brents, is a rare opportunity to exploit this connectivity.

The Listed (II\*) 15th century timber framed and jettied TS Hazard building (at present occupied by the sea cadets) is a natural focal point and is the main determinant of the scale of the area. The setting of this building must be taken into account in any proposals for this area. The Swan and Harlequin pub and 18th century townhouse adjoining it, effectively terminate the view from Conduit Street, and their location provides a cue for enhancing the sense of an informal square at this point. This area is discussed in the Faversham Streetscape Strategy, and the conclusions in that document are endorsed in this one.

Public access to the Town Quay and Green via TS Hazard and Quay Lane is somewhat confusing in that the ownership of the area is unclear.

This area has the greatest concentration of traditional paving materials in situ, and every effort should be made to ensure their survival and preservation in situ. They represent significant heritage assets and should be a reference point for public realm improvements. They consist of the stretch of granite setts and 'wheelers' leading from North Conduit Street to the green, lengths of setts on the eastern side of North Lane, the margin of blue glazed pavers at the junction of Belvedere Road and the kerbing along Quay Lane.

The northern portion of this area continuing the attractive fine-grained mixed use character of this sub area is primarily accessed from the narrow southern end of Belvedere Road. Pavements are narrow and discontinuous, although the widened Belvedere Road is characterised by pavements of a standardised design, including a standard road access to Provender Mill.

The only east-west link to Abbey Street between Quay Lane and Standard Quay is Smack Alley, a narrow passageway, hardly visible between the continuous historic facades of Abbey Street. The alignment of the alley gives a glimpse of the creekside from Abbey Street: its value as a key connection should be recognised. The recent construction of a crossing point at Belvedere Road is an acknowledgement of this importance. The surface of Smack Alley is tarmac, patched in places. Given the significance of this lane it could be re-surfaced in more locally distinctive materials.

The character of the quaysides in this area varies considerably. The dead end at its northernmost extent has been partially utilised as an informal sitting area. The 'missing link' at Provender Mill where the building fronts directly onto the creek has been connected by the excellent

example of fixing to the building a cantilevered deck supported by cast iron brackets.

The privately owned quayside just north of Town Quay currently occupied by commercial premises is protected at either end by security fencing. The quayside itself consists of a grassed strip approximately 3 metres wide fronted by a single storey timber building.

## 2.03 The Town Quay, Quay Lane and 'Square'. continued



fig 26



fig 27



fig 28



fig 29



fig 30

**fig 26**  
The boundary between developments runs across the quayside creating a dead end.

**fig 27**  
The inappropriate and poor condition of the surface degrades the setting of the listed building (II\*) T S Hazard on the right.

**fig 28**  
Discontinuity of access. Belvedere Quay looking south west.

**fig 29**  
Town Quay and green looking to the north eastern fenced boundary.

**fig 30**  
Quay Lane looking towards the creek from Court Street. Note the narrow pavements either side of the roadway.

**fig 31**  
A recent cantilevered deck on the front of Provender Mill creates a welcome connection along the quayside.



fig 31

## 2.03 The Town Quay, Quay Lane and 'Square'. continued

fig 32

The unsightly boundary of the sub station and the insensitive treatment of the levels on the south eastern corner of the medieval T S Hazard.

fig 33  
A rare survival: historic glazed brick paver margin at the junction of Belvedere Road and Quay Lane.

fig 34  
The over-wide suburbanised access to Provender Mill is inappropriate in this location. Note the van parked on the junction.

fig 35

The south western boundary of Town Quay and green, with the boundary wall of the car park and the bridge beyond.

fig 36

A glimpse down Smack Alley from Abbey Street reveals a direct link to the creek.



fig 32

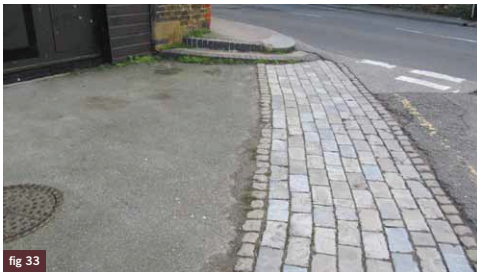


fig 33



fig 34

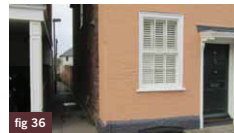


fig 36



fig 35

## 2.03 The Town Quay, Quay Lane and 'Square'. continued

fig 37

Town Quay and green from Front Brents, with the timber framed T S Hazard in the centre..

fig 38

Smack Alley looking towards Abbey Street illustrating the condition and materials of its surface.

fig 39

Smack Alley looking towards the creek.

fig 40

Belvedere Road looking towards Quay Lane; an opportunity for sensitive improvement.

fig 41

Conduit Street. Heavy lorries giving way and negotiating the corner hint at opportunities for traffic management.

fig 42

Conduit Road looking north east to the 'square', terminated by the Swan and Harlequin. The unsightly boundary on the right and degraded surface on the left should be improved.

fig 43

The well preserved and finely detailed granite accessway to Town Quay from Conduit Street.



fig 37



fig 38



fig 39



fig 40



fig 41



fig 42



fig 43

## 2.03 The Town Quay, Quay Lane and 'Square'. continued

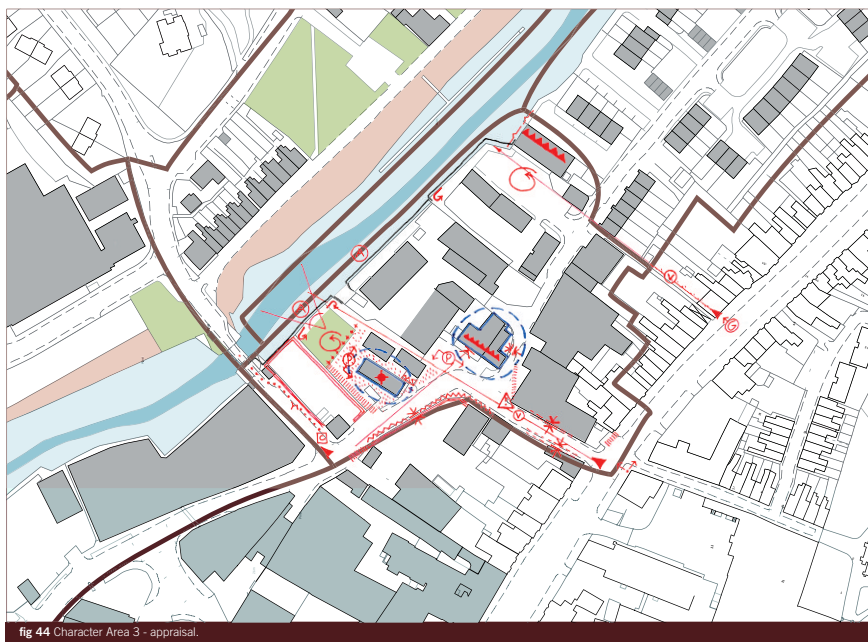


fig 44 Character Area 3 - appraisal.

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## 2.04 Front Brents and the Bridge. Character area 4.

The main approach to Front Brents from the town is via the creek bridge: this is a gateway to the north bank of the creek from the town centre. It also forms a key east-west connection between Front Brents and the Basin.

The bridge is a 'pinch-point' funnelling pedestrians and vehicles (which have to give way to oncoming traffic on the single lane carriageway) on this narrow crossing over the sluice gates (fig 45).

As the swing bridge has not been operational for some years, the working areas and related small scale buildings are in need of repair and maintenance (fig 46). The gates on either side which closed the road when the swing bridge was open are in a very poor state of repair. The road deck tarmac surface has holes in places, revealing the steel deck below.

The 'kee clamp' type galvanised steel railings are of the most basic type and do not reflect the character of this area (fig 47). The pedestrian experience is somewhat hazardous, not only due to the very narrow pavements and heavy vehicles passing over the bridge, but also on crossing Bridge Road.

The link between Front Brents and the path running along the north side of the Basin is indistinct and obscure where it would cross the grassed area (fig 48). Indeed the designated footpath leads the pedestrian along Brent Road, a much less attractive

route. It is necessary that both visual and physical connectivity is improved to link Front Brents and the north side of the Basin.

Front Brents itself is an attractive creekside walk, fronted by two short terraces of modest, rendered, 19th century artisan cottages, alternating with intimate greens (fig 49). It is entered from Bridge Road past a gate which acts as an emergency access way and emphasises its status as a private road (fig 50). An information board explains the significance and history of The Brents, but it has suffered some vandalism.

The approach to Front Brents is by way of a plastic grid stabilising the ground, as this is an area of vehicle turning (fig 51). The walk is wide enough for residents parking, although this is causing physical and visual intrusion onto the banks of the Creek, but is 'contained' somewhat by a line of timber posts.



fig 45

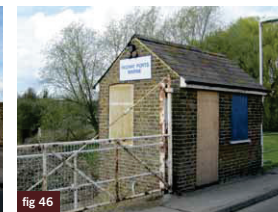


fig 46

fig 45  
A glimpse of Front Brents from Bridge Road, framed between two disused structures as the narrow bridge is approached.

fig 46  
A disused building on the north side of the bridge. See also the poor condition of the gate.

Faversham Creek Streetscape Strategy

## 2.04 Front Brents and the Bridge. continued

The shared surface of Front Brents extends directly to the front doors of the cottages (fig 52), although further east there are some short front gardens. The surface is tarmac with a surface dressing of gravel which in its mature state is attractive as well as durable. However recent 'blacktop' patching of potholes is less attractive.

Timber posts, granite kerbs set vertically into the greens and concrete posts, all contribute to keeping the greens and river banks free from the intrusion of vehicles (fig 53). The small car park for 10 vehicles (fig 54), set within the larger green (fig 54), is adequately designed, but obviously the demands of residents parking exceeds this capacity, especially as the Albion pub, standing alone at the centre of Front Brents, is adjacent to it (fig 55).

The diagonal path across this larger green (fig 56) is a well used route to the bridge, avoiding the traffic on Church Road and opening up classic views of the Faversham waterfront (fig 57 and fig 58), including Town Quay. Two cast iron framed 'serpent' pattern benches are situated in this sunny position. A home made bench nearer the bridge reflects the desirability of stopping a while in this

area. Generally, seats and benches are fairly scarce around the Creekside area (fig 59).

The northern fringe of Front Brents character area is marked by Church Road, here the pavements are very narrow in places, sometimes a little below the road level, and therefore are less attractive for the pedestrian. The pavement disappears altogether as it runs into the large green running down from Upper Brents (fig 60). The surfaces of these pavements are generally tarmac.



fig 47



fig 48

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fig 49

**fig 47**  
Approaching Front Brents from the bridge, also showing the inappropriate railings.

**fig 48**  
The visual link from the end of the basin pathway to Front Brents, across Bridge Road.

**fig 49**  
The informal character of Front Brents, with buildings and small gardens alternating.

## 2.04 Front Brents and the Bridge. continued



fig 50



fig 51



fig 53



fig 54



fig 52



fig 55

**fig 50**  
The entry point to Front Brents, with information board.

**fig 51**  
A ground reinforcement grid provides a makeshift turning area, although erosion of the surface continues.

**fig 52**  
The surface of Front Brents extends to the fronts of most houses.

**fig 53**  
The edge of the greens is protected in this case by upturned granite kerbing. Note the 'serpent' frame benches in the background.

**fig 54**  
The car park and vehicular entrance to Front Brents with the disused church in the background.

**fig 55**  
The frontage of the Albion pub provides a sitting out area directly onto Front Brents.

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## 2.04 Front Bents and the Bridge. continued

fig 56

The diagonal path from Front Bents to Church Road provides a pleasant route to the bridge.

fig 57

The spire of Faversham Church framed between buildings on Belvedere Wharf, seen from Front Bents.

fig 58

An attractive view of Town Quay from Front Bents.

fig 59

The north end of Front Bents showing erosion of the edge.

fig 60

The narrow pavement along Church Road terminates abruptly as it adjoins Upper Bents.



fig 58



fig 56



fig 57



fig 59



fig 60

## 2.04 Front Bents and the Bridge. continued



fig 61 Character Area 4 - appraisal.

## 2.05 Belvedere Road. Character area 5.

This area, north of Provender Mill, is solely comprises recent (1980s – 90s), two and three storey residential terraces, most fronting the Creek and others fronting the east side of Belvedere Road. The west side of Belvedere Road is mainly fronted by garage courts, relieved by the gable ends of garage blocks. Thus, in contrast to the Town Quay area, the character of this area is of single use, similar building types, constructed within a relatively short period

The massing of the developments creates a rhythm of grouped gable ends alternating with ridgelines parallel with the quay- and a large pyramidal roof topped by a diminutive cupola.

The only truly public realm is Belvedere Road: five short access ways link the road to the quayside, offering views of Front Brents and Crab Island. Looking from Front Brents, these gaps provide vistas to the church spire and tower. The northern end of the area is at present terminated by a partially vacant site extending across the carriageway, with the only way through being an extension of the eastern pavement of Belvedere Road (*fig 61*).

Along the creek, there is a quayside walk in front of the houses. This is continuous, except in one location where the boundary wall between developments extends to stop up this route. The quayside walk is not formally adopted but is used informally (*fig 62*). The surface is of brick pavers laid in a herringbone pattern typical of the period. The quayside margin is protected by bollards linked by chains (*fig 63*). The quayside front gardens and other boundaries introduce a suburban character, somewhat at odds with the current character of creekside overall.



fig 62

**fig 62**  
Looking north along Belvedere Road where it terminates in a vacant site. However, the pavement continues northwards on the right.

**fig 63**  
The southern end of the quayside of this area is terminated by a boundary wall, creating a dead end.

**fig 64**  
View showing the general character of this area, note the blockwork surface of the quayside and the front gardens.



fig 63



fig 64

Faversham Creek Streetscape Strategy

## 2.05 Belvedere Road. continued

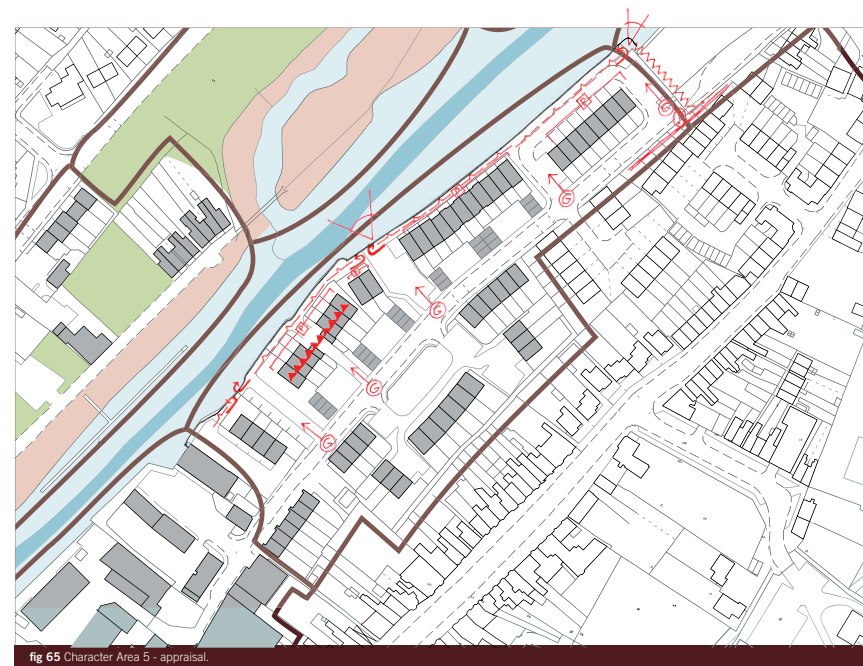


fig 65 Character Area 5 - appraisal.

Faversham Creek Streetscape Strategy



## 2.06 Crab Island. Character area 6

The Crab Island area is the most open of the creekside areas within built-up Faversham. It has two distinct parts: the high area sweeping down from Upper Brents in a grassed swathe with occasional trees (*fig 65*), and the lower, wilder creekside bank, edged by reeds and mud (*fig 66*). A narrow tributary creek divides these two areas. An indentation in the line of the bank was formed to aid the turning of barges.

The creekside footpath is the only delineated pedestrian route. It is approached from the west via Front Brents through a flimsy timber post and rail fence; the unmade path is subject to flooding at particularly high tides.

An interpretation board is situated alongside the path. The information panel is missing and the timber frame for the board is not in good condition (*fig 67*).

The outlet for the tributary is formed by corrugated steel culvert arches (*fig 68*). These carry the path and earth bank over the inlet. This 'bridge' is unattractive and could be improved.

The path meanders follows the creek bank until it makes an abrupt turn inland as it reaches the concrete retaining walls of the Faversham Reach development whose flank walls dominate this area (*fig 69*). Walkers use the footpath here to reach the creekside (*fig 70*).

The footpath, now slightly raised and directly following the concrete wall, turns north eastward to end in a dank, dark, cramped alley between the concrete wall and industrial premises (*fig 71 and fig 72*). The northern boundary of the area is formed by Upper Brents Road, often lined by parked cars; there is no footpath on this side of the road.



fig 66

**fig 66**  
View across the green swathe from Upper Brents towards the church spire.

**fig 67**  
The path across Crab from the north easterly end of Front Brents.

**fig 68**  
The timber frame for the interpretation board remains, but the board itself is missing. The informal character of the path is missing.



fig 67



fig 68

## 2.06 Crab Island. continued

**fig 69**  
The unsightly corrugated steel lining for the culvert and outlet, the footpath runs over it.

**fig 70**  
The footpath meanders north east towards the retaining walls of the Faversham Reach development.

**fig 71**  
Attractive view of the creek from the path, looking towards the far end of Waterside Close, the masts of the barges and the open landscape of the Swale beyond.

**fig 72**  
The path continues inland along the foot of the concrete retaining wall.

**fig 73**  
Turning the corner, the path continues on the north side of the retaining wall to the rear of industrial premises.



fig 71



fig 69



fig 70



fig 72



fig 73

## 2.06 Crab Island. continued



fig 74 Character Area 6 - appraisal.

Faversham Creek Streetscape Strategy

## 2.07 Standard Quay. Character area 7

This waterfront area is the main location where Thames barges can be seen, either moored or on stocks being repaired. It is both a working area and an emblematic element of the heritage of Faversham (fig 75).

**fig 75**  
Significant areas of Standard Quay are devoted to the repair and restoration of the locally distinctive Thames barges which have traditionally traded from Faversham.

**fig 76**  
Informal sign at the southern end of Abbey Street advertising the attractions of Standard Quay.

**fig 77**  
The unpromising approach to Standard Quay, turning left past The Anchor pub on the right.

**fig 78**  
An attractive informal group of weather-boarded buildings in front of the timber framed and brick 'Monks Granary' on the right.

**fig 79**  
The narrow path which serves as an alternative approach to the Standard Quay area from Belvedere Road. Largely disused sites can be seen on the right.



fig 75

To some degree it is a place which has to be discovered, either by walking some nearly half a mile from Market Place along the full length of Abbey Street (fig 76), or by entering via the uninviting narrow footpath at the top of Belvedere Road. In both cases it is not until one has finally reached the quay, that its attractiveness and character is revealed. The Anchor pub terminates the long view north up Abbey Street. It is here that a number of signs of a variety of shape, size and design announce the businesses which have been established in recent years. Even having turned left at the Anchor, Standard Quay remains hidden; instead one is confronted by the

entrance to the bus depot and the 1950s Quayside House single storey offices (fig 76). The road surface at this point is poorly maintained and a sign board indicates other business premises. Finally, having ventured a little to the right, the masts and tan sails, and the roofscape of the wharfside buildings come into view (fig 78).

The approach from Belvedere Road is particularly unattractive as the narrow gravel path is enclosed between a retaining wall and the temporary fencing of the yard, and disused sites (fig 79).

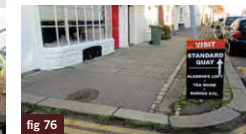


fig 76



fig 78



fig 77

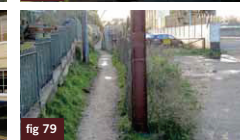


fig 79

Faversham Creek Streetscape Strategy

## 2.07 Standard Quay, continued

The working front of Standard Quay is a concrete apron whose aggregate finish gives it a sandy appearance which is not unattractive. The space between the black weather-boarded former buildings facing the quay and the Monks Granary is by contrast potholed tarmac (fig 80). This area accommodates some car parking and sitting out space for the café and shops.

Beyond this area the Georgian period Standard House terminates and deflects the view to the north. This handsome building, long vacant, is undoubtedly a focal point for the area (fig 80). Its stone steps are in need of repair and the unmade-up surface

in much of this part of Standard Quay can accommodate only small volumes of vehicular movement. The area up to the other landmark, the former warehouse, Oyster Bay House, is at the time of writing undergoing change and therefore precludes appraisal. The footpath at present, unmade-up and cramped, leads on the landward side to the Chambers Wharf area.

At the southern end of this character area, occupied at present by a coach depot and former oil depot, public access is extremely limited. The quayside narrows considerably at this point, terminating at a dead end looking onto an indented waterfront, precluding any pedestrian

connectivity to the northern end of Provender Walk. This is the longest 'missing link' on the southern side of the creek. The only pathway is on the landward side, from Belvedere Road. The concrete surface of this narrow quay incorporates some rail track, a reminder of the industrial heritage of Standard Quay. These should be recognised as heritage assets (fig 82).



fig 80



fig 82

fig 80

The poor state of the surface detracts from the setting of these historic buildings.

fig 81

Two landmark buildings at the northern end of Standard Quay: Standard House dating from the late Georgian period and Oyster Bay House, a 19th century warehouse. Both should influence the materials and design of the surfaces of Standard Quay.

fig 82

The southern end of Standard Quay narrows considerably. The remnant rails set in the concrete surface are a valuable reminder of the working heritage of the Quay.



fig 81

Faversham Creek Streetscape Strategy

## 2.07 Standard Quay, continued

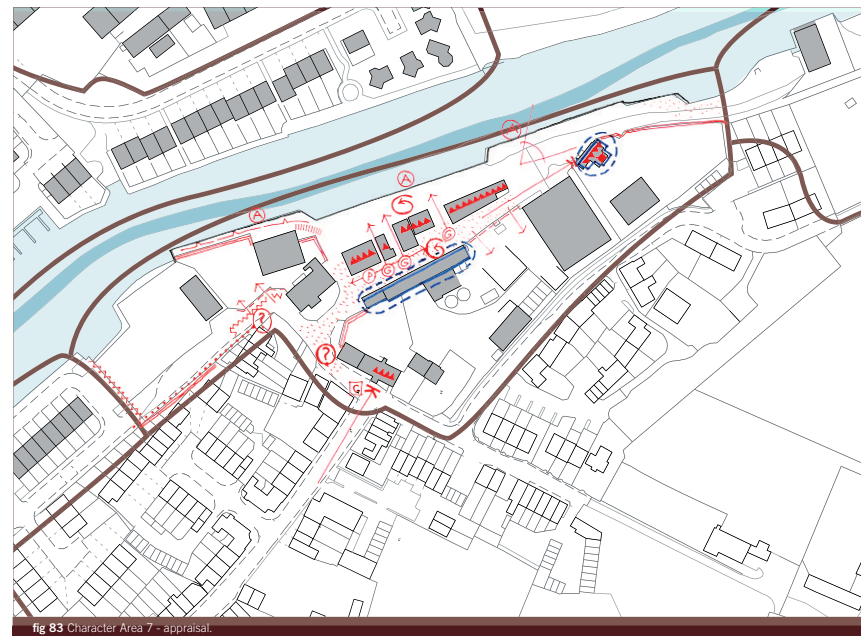


fig 83 Character Area 7 - appraisal.

Faversham Creek Streetscape Strategy

## 2.08 Faversham Reach and Waterside Close. Character area 8.

This is a relatively recent gated residential development with terraced groups of houses fronting the Creek (fig 83). The natural slope of the land has been built up behind the sheet piling to create a quayside with a small marina. A waterfront footpath has been provided as the development was designed for public access (fig 84). The freestanding houses at the extreme east of the development are designed as a landmark cluster at the end of the built up area of Faversham's north bank. The waterfront footpath ends abruptly at the eastern and western extremities of the development (fig 85). The roadways serving both

developments link northwards to Upper Brents via an access area shared with the industrial estate (fig 86).



fig 85



fig 86



fig 87

**fig 84**  
The northerly part of the development with a walkway in front of the gardens. The small marina can be glimpsed to the left.

**fig 85**  
Almost the whole of the waterfront of the development showing the walkway in front of the brick boundary walls

**fig 86**  
The south western boundary of the area protected by security fencing.

**fig 87**  
The area shares its access with the industrial estate, marked by imposing gate piers and interesting 'guardhouse' to the left. The existing footpath emerges on the right hand edge of the picture.



fig 84

## 2.08 Faversham Reach and Waterside Close. continued



fig 88 Character Area 8 - appraisal.

## 2.09 Chambers Wharf and Iron Wharf. Character area 9.

The area including and beyond the magnificent, isolated Oyster Bay House warehouse is otherwise bereft of formal buildings. Instead it is occupied by the storage of yachts and former railway freight wagons minus their wheels, so numerous as to be the local vernacular building form. The wagons, generally used as storerooms are here, because the rail spur serving the wharfs spread into this area, giving it the triangular plan seen on the map (fig 89).

The designated footpath passes through the area alongside the wharf and onto the open flatlands leading to the Swale. Otherwise the area is largely private and is traversed by unmade-up tracks (fig 90).

An area which has some potential for enhancement is that immediately to the north of Oyster Bay House (fig 90). This is the 'entry' point to the area from the south via an unmade footpath and is more open than the rest of the area. From here the footpath continues along the wharf whilst another path turns inland. However the need for security for

boat owners and the working nature of the area would appear to indicate that options for improvement are likely to be limited.



fig 89

fig 89

Numerous former railway freight wagons used for storage, are part of the informal character of the area.

fig 90

A recent narrow unmade path running to the rear of the quayside frontages.

fig 91

The north side of Oyster Bay House (on the left) which has the potential to be a focal space for the area.



fig 91

Faversham Creek Streetscape Strategy

## 2.09 Chambers Wharf and Iron Wharf. continued

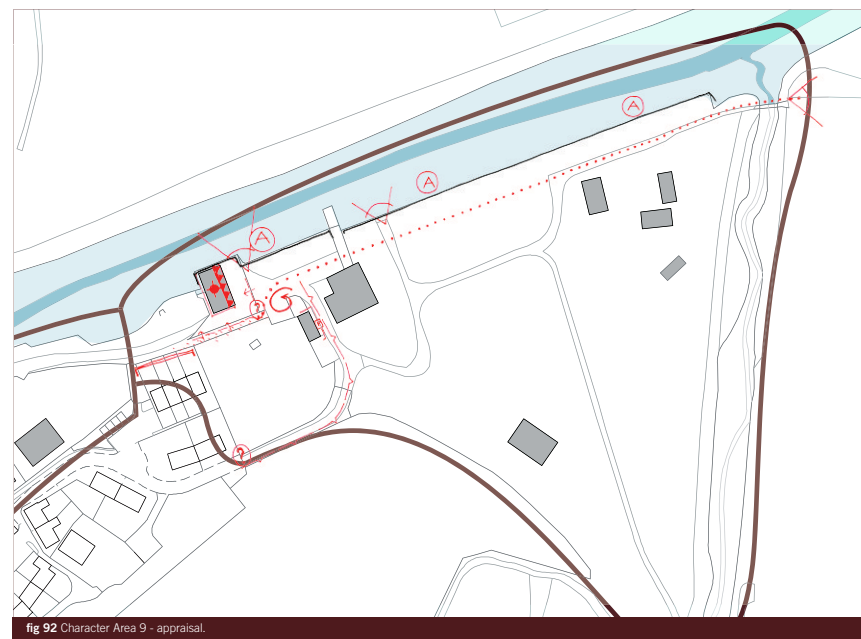


fig 92 Character Area 9 - appraisal.

Faversham Creek Streetscape Strategy

## 2.10 The Industrial Area and North East Fringe. Character area 10

Due to the nature of the industrial estate, an enclosed area with service roads and yards associated solely with the industrial units, this zone lies outside the remit of the strategy. However, it shares its access point at the eastern end of Upper Brents (*fig 93*). A public footpath skirts two thirds of its perimeter, which literally marks the extremity of the built up area.

The footpath which skirts the northern perimeter of the Faversham Reach development, described in 6.08 emerges from its unpleasant dogleg route at the entrance to the industrial estate (*fig 94*). When vehicles are parked in the area it is unclear as to where the footpath resumes, as the very small footpath signs are obscured. A narrow gap between the Industrial estate's

western boundary and adjacent housing (*fig 95*) reveals a path which suddenly reveals wide, open, flat fields (*fig 96*). The footpath, now an unmade track leads round the boundary of the estate on its northern and eastern sides. A gate between fields on the eastern side concentrates northern and eastern sides. A gate between fields on the eastern side concentrates footfall,



fig 94



fig 95

producing a very muddy surface in the winter months (*fig 96*). Eventually the footpath links with the sea wall running eastward along the creek. The most easterly boundary of the Waterside Close development is an unsightly collection of fencing, possibly erected to protect against intruders (*fig 97*).



fig 93

**fig 93**  
Eastern end of Upper Brents, with the entrance turning to the industrial estate on the right and the footpath continuing in line with the houses on the left.

**fig 94**  
The entrance to the industrial estate to the left. The formal footpath diagonally crosses this unsightly, ambiguous space.

**fig 95**  
The path skirts the industrial estate to the right towards the open fields.

## 2.10 The Industrial Area and North East Fringe. continued



fig 96



fig 97



fig 98

**fig 96**  
Once at the open fields, the path becomes two informal tracks.

**fig 97**  
The gate midway along the north eastern boundary of the area concentrates footfall, creating a muddy area in wet weather.

**fig 98**  
The boundary of Waterside Close, with the marked path continuing atop the sea wall to the left.

## 2.10 The Industrial Area and North East Fringe. continued



fig 99 Character Area 10 - appraisal.

Faversham Creek Streetscape Strategy

## 3.00 Creekside Streetscape Issues

The appraisal of the character areas in section 2.00 has underlined the variety of environments which comprise the creekside area, offering a range of views, senses of enclosure from open to enclosed, or urban to semi rural, activity to tranquillity and a waterside character influenced by the state of the tide and the passing seasons.

Section 2.00 has also pinpointed a variety of paths, pavements and places reflecting the character of the creekside. In very broad terms, the streetscape can be divided into the harder areas and softer areas.

However as each character area has been appraised a number of problems and issues have been identified. They relate for example to poor maintenance, incoherence, potential hazards, or inappropriateness of parts of the streetscape.

These are grouped into the topics below. They form the basis for the strategy and projects described in the following sections of this strategy.

## 3.01

The problems are grouped into the topics **(A) – (H)** below, (some of which overlap in some locations):

- A** Poor legibility and signage.
- B** Lack of continuity in routes.
- C** Hazardous routes.
- D** Indirect or poorly aligned routes.
- E** Poor or badly maintained surfaces.
- F** Unattractive surfacing, degrading the area character or the setting of heritage assets.
- G** Ambiguous spaces (public or private?)
- H** Street furniture in poor condition or non existent. Selected photo images in the following sections illustrate some of the issues.

Faversham Creek Streetscape Strategy

## 3.00 Creekside Streetscape Issues. continued

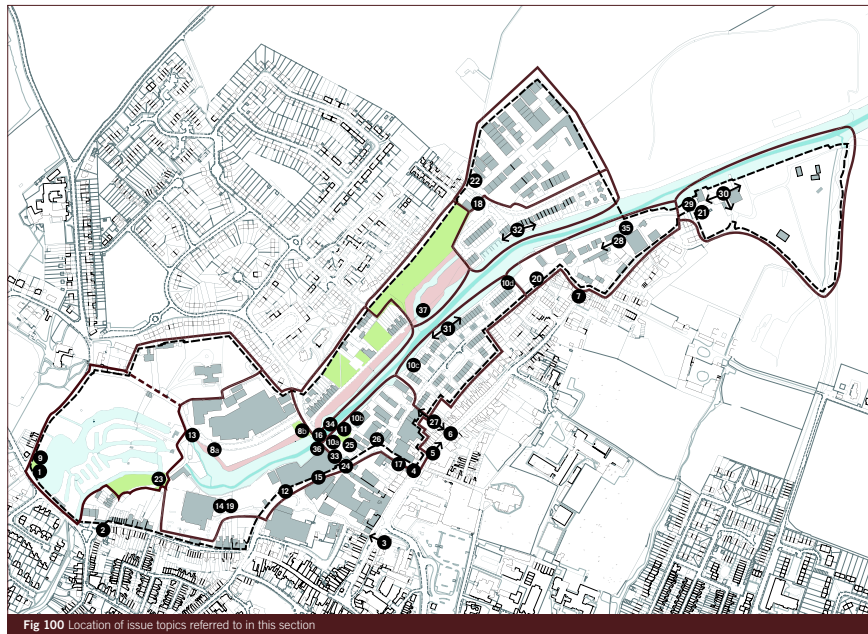


Fig 100 Location of issue topics referred to in this section

Faversham Creekside Streetscape Strategy

## 3.00 Creekside Streetscape Issues. continued

**A****Poor Legibility And Signage**

The 'gateways' to the creekside are largely not apparent, unclear and lack signage. Specific instances include:- (numbers relate to locations on map (fig 100))

- 01 The small green space at the junction of Davington Hill and West Street. This provides an excellent and rare view of Stonebridge Pond and has an interpretation board and seat, but lacks any link to the footpath routes overall. It is also a dead end - point (b).
- 02 Flood Lane is another attractive route to the Pond, and to the Basin, but there is no sign at the junction of West Street.
- 03 Partridge Lane is the nearest potential link from Market Place, but lacks any sign and after the first 35 metres becomes unattractive and indirect. (fig 101)
- 04 Quay Lane is a key potential link; an existing fingerpost on the opposite side of Court Street needs reinforcing at the Quay Lane corner. Although there is an excellent sight line, the pavements are extremely narrow and on the north side have hazardous steps. The heavy continuous traffic adds to the hazardous feeling (illustrating point c)
- 05 Abbey Street looking north at Quay Lane. The 'unofficial' signage and 'A' boards point towards Standard Quay, but this needs permanent signage and repeaters, at one or two points along this long albeit attractive street.
- 06 Smack Alley, Abbey Street. A sign would reinforce this almost invisible yet direct route. The recent crossing at Belvedere Road at the other end of the route is helpful, but signage would assist legibility at this point.
- 07 Abbey Street north end. Unofficial and informal signs perform the task of leading people to Standard Quay which is not visible at this point, and some of the best views of creekside Faversham are denied. (fig 102 and fig 103)



fig 100



fig 102



fig 101

fig 101 The junction of Partridge Lane and Court Street. A potential link to the creek although it is not apparent. (ref to point 03)

fig 103 Another sign at the same location. (ref to point 07)

fig 102 Sign at the north end of Abbey Street to inform and attract visitors. (ref to point 07)

Faversham Creekside Streetscape Strategy



## 3.00 Creekside Streetscape Issues. continued

**B**  
**Lack Of Continuity Of Routes**

There are a number of locations where footpaths lead to a dead end, or a pavement or footpath ends in a roadway, often heavily trafficked. Examples include:-

08  
Either end of the footpath along the north side of the Basin. At the western end the footpath discharges the pedestrian onto Brent Road just where it narrows. At the eastern end, the path ends in an area of grass, with little visual continuity with the path along Front Brents.

09  
The viewing area at Davington Hill and West Street, see (a) above.

10  
Various points along the quaysides on the town side of the Creek:-

- between Town Quay and the bridge
- between Town Quay and the quayside on its eastern side
- between Belvedere Wharf and Provender Walk
- between north end of Provender Walk and southern end of Standard Quay (occupied by the oil depot and bus depot).

These could be implemented through negotiation with the landowners or by

planning agreements as appropriate, to achieve a continuous walk from Chambers Wharf to the bridge.

11  
Town Quay/Green. There is no link to the north east or south west or. Potential routes are blocked by a wall and a security fence.

12  
North Lane, east side towards the junction with Partridge Lane. (See also (c) below.)

**C**  
**Hazardous Routes**

Discontinuous, narrow or illegible routes often tend to be hazardous in terms of potential pedestrian/ vehicle (sometimes heavy vehicle) conflict, or in terms of perceived threat to personal safety. Critical examples of hazardous routes are (generally from west to east):

13  
The junction of Brent Road and Flood Lane

14  
The path between Morrisons and Shepherd Neame's service yards

**fig 104**  
The pavement ends at a hazardous point in North Lane/Conduit Street. (ref to point 15)

**fig 105**  
The footpath running along the northern boundary of Faversham Reach ends with a blind corner. The path is in permanent shade and the surface is not well maintained. (ref to point 18)

**fig 106**  
The approach road to Morrisons Store. The footpath to the creek basin is located at the gap in the brick wall on the right. (ref to points 14 & 19)



fig 104



fig 105



fig 106

Faversham Creek Streetscape Strategy

## 3.00 Creekside Streetscape Issues. continued

15  
Conduit Street/North Lane including the junction with Partridge Lane. The pavement also stops on the east side of Conduit Street. (fig 104)

16  
The bridge: tidal flow traffic and very narrow footways.

17  
Quay Lane: narrow pavements and curving step at Belvedere Road junction.

**D**  
**Indirect, Poorly Aligned Routes.**

18  
The footpath on the north side of Faversham Reach development. This is inconvenient, unattractive, poorly maintained and potentially hazardous. (fig 105)

19  
The footpath between the service yards of Morrisons and Shepherd Neame's. As a dogleg route this is almost invisible, unattractive and with a local reputation for being hazardous. The steps at either end further reduce its convenience. (fig 106)

20  
The footpath at the northern end of

Belvedere Road, behind the oil depot sites.

21  
The footpath south of the Oyster Bay House.

22  
The indistinct area around the entrance of the industrial estate and east end of Upper Brents.

**E**  
**Poorly Maintained Surfaces.**

23  
Flood Lane northwards to Brent Road. (Also relates to (f) below at Purifier building).

24  
Decorative paving and planting at the junction of Conduit Street and Bridge Road is in poor condition. (fig 107)

**F**  
**Unattractive Or Inappropriate Surfacing, Degrading The Area Character Or The Setting Of Heritage Assets.**

25  
The surfacing around TS Hazard building (broken tarmac etc), and the traditional paving to its west. (fig 108)



fig 107

**fig 107**  
The decorative paving and planting at the corner of Bridge Road and North Lane is uncared for. Perhaps the paving materials could be rescued and reused elsewhere. Note the clutter of unrelated signs etc. (ref to point 24)

**fig 108**  
Poor choice of materials around TS Hazard. The area is also neglected. (ref to point 25)



fig 108

Faversham Creek Streetscape Strategy

### 3.00 Creekside Streetscape Issues. continued

26 The parking area in front of the Swan and Harlequin pub.

27 Smack Alley: nondescript surfacing along a historic lane.

28 The area on the south side of the weather boarded buildings at Standard Quay.

29 The south side of Oyster Bay House: poor quality materials including road planings.

#### **G** Ambiguous Spaces

These spaces or routes are in some cases designated footpaths on private land, or permissive paths, or subject to agreement with the owners have the potential to become footways as creekside walkway exists.

30 The Chambers Wharf area

31 Provender Walk

32 Faversham Reach/Waterside Close

#### **H** Street Furniture In Poor Condition Or Non Existent.

The limited street furniture that exists in the area is generally in a poor state of repair. There is little seating, except along Front Brents and a lack of litter bins in places.

33 Poorly sited and little used seats on Bridge Road, either side of the bridge. (fig 109)

34 No seating at Town Quay and green.

35 Informal, unfixed seating at Standard Quay as none is provided.

36 Railings at the bridge are poorly designed and in poor condition. Gates and other sluice and bridge equipment are in poor condition. (fig 110)

37 The interpretation board at Crab Island is missing and the timber frame for the board is in poor condition. (fig 111)



fig 109



fig 110



fig 111

**fig 109**  
Steel seat set low in the pavement tarmac in an unattractive location in Bridge Road. (ref to point 33)

**fig 111**  
Missing interpretation panel; the timber frame is also in need of repair. Perhaps these could be relocated? Crab Island (ref to point 37)

**fig 110**  
Equipment at the bridge in poor condition. The galvanised steel railings could be replaced with a more appropriate design. (ref to point 36)

### 4.00 Strategy for the Creekside Streetscape

#### 4.01 Overall Principles

The following overall principles should be applied to the public realm of the Faversham creek area and form part of the strategy for the creekside area. These principles should be referred to when implementing the plan, individual projects and development management decisions. They should also form the basis for any briefing for development sites regarding the public realm, frontages and access to the creekside area.

4.02 The aim of all improvements for the creekside public realm should be to create a coherent, connected, legible, attractive, accessible and safe footpath network within the area and between it and other parts of the town.

4.03 The design solutions should contribute to the enhancement of a sense of place and local distinctiveness and reflect the character of the conservation area (and its sub areas).

4.04 Design solutions should improve the setting of heritage assets as defined in this document.

4.05 Improvements should be functional, durable, fit for purpose and designed for ease of maintenance.

4.06 Improvements should be designed to be implemented on a piecemeal yet coordinated basis, as resources allow.

4.07 Street furniture (railings, bollards, seats, bins, lighting etc) should be appropriate to the character of the creekside. Street furniture should also be suited to the overall character of the town and the particular character of each character area. Where street furniture is specified in the Faversham Streetscape Strategy this should normally be considered as appropriate.

## 5.00 The Plan

### 5.01

#### Introduction

Section 4.00 outlines the guiding principles regarding the improvements to the streetscape of the creekside area. This section establishes guidelines for the planning and design of specific items in the overall streetscape. Section 6.00 sets out guidance for projects for discrete areas of the creekside.

Design and implementation decisions regarding the creekside must be influenced by the particular context to which they relate. As section 2.00 has demonstrated, the creekside area has a number of sub areas, each with their own character. These can be broadly divided into the 'Town side' areas and 'Brents side' areas. The following two design codes outline the general approach to be taken in each. This will determine choice of materials and the relative informality or formality of design decisions.

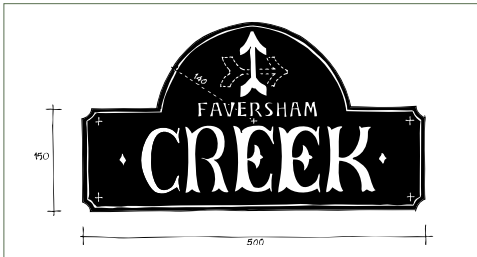


fig 112 Suggested design for creek signage. A variant of the existing street nameplates.

### 5.02

#### Town Side Design Guidelines.

In general the southern side of the creek extending from Iron Wharf in the east to Ordnance Wharf in the west, consists of a more built-up harder edged environment knitting back to the fabric of the historic town centre. Almost without exception it is on this southern bank where quayside and wharf are to be found, sometimes with buildings directly on the waterfront. The area has a relatively tight grain and the public realm is usually hard surfaced.

Consequently, paving should be more formal, for instance using paving slabs, setts and kerbs where appropriate, signage should be fixed to walls and cast iron should predominate over timber.

### 5.03

#### Brents Side Design Guidelines

Generally the northern bank of the creek, extending from Stonebridge Pond (including the east end of Flood Lane), to The Brents, Crab Island and the north eastern fringe of the creekside area. This is characterised by a landscape of greens, tributary creeks, soft banks, trees and informal paths, both in form and materials. Exceptions to this pattern are;

the Front Brents modest terraces (albeit here the frontages abut shared spaces) and the recent Faversham Reach and Waterside Close residential developments which present the only formal quayside on this bank.

Consequently, the design approach should be more informal in character, with 'soft-edged' paths of a gravel-type appearance, timber posts delineating routes as necessary and less lighting overall. Signage is likely to be located on posts, and timber will predominate over metal. However, the only exception to this recommendation in the area is on the north side of the bridge, where railings, 'serpent' seats and lamp posts would be appropriate.

### 5.04

#### Signage

The Appraisal has identified the relative incoherence and illegibility of the footpaths, public and semi public spaces within the creekside area, as being an inhibitor to greater access for residents and visitors alike. It is proposed that a variant of the existing street nameplates as specified in the Faversham Streetscape Strategy be used to (a) direct pedestrians to the creekside

## 5.00 The Plan. continued

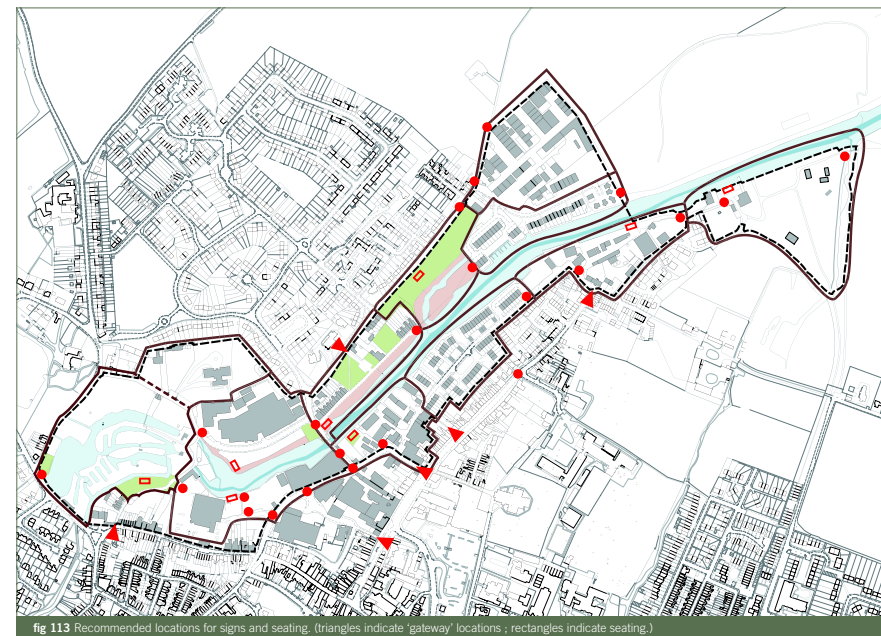


fig 113 Recommended locations for signs and seating. (Triangles indicate 'gateway' locations; rectangles indicate seating.)

## 5.00 The Plan. continued

and (b) provide signage to mark the footpaths and accessible quaysides around the creek.

The drawing (fig 112) shows the design which utilises the font and general nameplate shape, but which incorporates space for a direction arrow in a semicircular panel. The overall shape of the nameplate is reminiscent of traditional signage and the arrow which would be pointing in one of four positions: ahead (upwards), left, right and left and right.

The sign (500mm wide) should be of an enamel finish on a metal base, with the background colour being complementary to the town centre nameplates which are dark blue. Signs should only be used where absolutely necessary, to indicate the pathway (map fig 113) shows suggested locations for signs, etc. Wherever possible the nameplates should be fixed to walls. When the creekside sign is located in conjunction with a street nameplate, it should be located on the vertical centreline of the street nameplate, either above or below it.

Where wall fixing is not possible, the nameplate should be fixed to a lamp post or other existing post. Only if this

option does not exist should a new post be provided for the nameplate. It may be possible or appropriate in certain locations to mount the nameplate (double sided) as a hanging sign from a bracket.

## 5.05

**Codes for Street Furniture,**

Interpretation, Lighting and Fittings  
The appraisal has identified where items of street furniture, interpretation, lighting etc are in poor condition, are inappropriate, create clutter, or would be beneficial if installed. These guidelines are set out as a reference point when considering the design and location of such items. As a general rule items should be as robust as possible in terms of materials and installation, should be located having regard to each other (e.g. seat, bin, light and sign) and avoid incremental collections of items creating clutter.

**(a) Street Furniture**

Seating. There are a number of locations (see map fig 113 above) where a seat may be welcome as a resting place or where a good view may be enjoyed. As a general rule the 'Serpent' pattern seat should be used (fig 114). This is identified in the



fig 114

fig 114

The 'serpent' design for seating. Cast iron frame with hardwood seat and back.

fig 115

Litter bin. A simple robust design.

fig 116

Lamp standard with swan neck bracket. Simple yet elegant. This is used in New Creek Road.

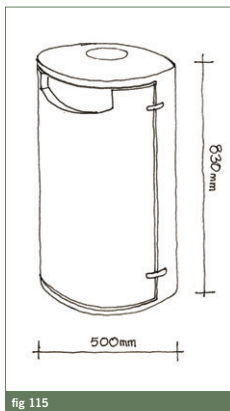


fig 115

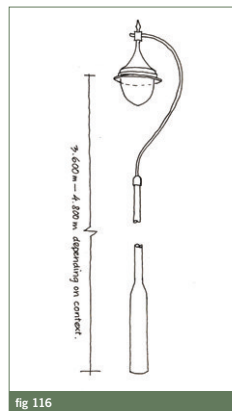


fig 116

## 5.00 The Plan. continued

Streetscape Strategy (2005) as a local pattern. The cast iron serpent shaped frame should be used with hardwood slats of appropriately robust section and appropriately treated. In the more 'rural' parts of the creekside, for instance in the Crab Island area or the area to the north and east of the industrial/employment area, purely hardwood timber may be considered more appropriate.

Litter Bins of a simple, cylindrical, cast metal type (fig 115) should be used, to replace the square, rounded corner standard 'heritage' type currently in use, when these need replacing. A wall mounted semi cylindrical variant could be used

where appropriate. Bins should be located in close proximity to seating, or in areas where people are likely to congregate or stop for a while. The colour for street furniture should be black. The Faversham coat of Arms could be used cast into casings if required, although any other markings or lettering would be superfluous.

**(b) Interpretation**

Interpretation boards are to be seen in a few locations. These are inevitably subject to damage and fading. Where it is desirable to replace these with new boards, these should be screen printed onto a metal base fixed to a robust timber frame, or preferably onto a wall where

appropriate. Key locations should be: The Town Quay, Front Brents, Stonebridge Pond/Flood Lane and Standard Quay. Elsewhere these should be used sparingly.

The existing guide/leaflet 'Faversham Market Town of Kings' identifies some points of interest in the creek area. Perhaps a future edition might be expanded to provide a trail around creekside. The busyFaversham.org website contains information on local walks, and a creekside trail could be added to that.

The feasibility of loading the data produced onto a smart phone app should also be explored. The experience of other towns and cities

developing this source of information points to its value in reaching a more diverse 'audience'.

**(c) Lighting**

Lights should be used where issues of personal safety and property security are considered as critical, or where the pedestrian areas are likely to be used as part of creekside activity, for instance along working quays. The height of the lighting unit should be appropriate to the pedestrian and the intimate scale of the creekside area; generally no more than 4 metres above street/quay level.

The lamp standard pattern (fig 116) may be suitable, painted black. On



fig 117



fig 118

fig 117

Mooring bollard, Town Quay.

fig 118

Cast iron bollard with the Faversham Town crest.

## 5.00 The Plan. continued

working quaysides, robust bulkhead lights may be an appropriate alternative to lamp standards in constricted locations. In all cases a 'warm white' lighting colour should be chosen; orange colours should be avoided. LED fittings will be particularly efficient.

**(d) Fittings and posts etc.**

Mooring rings, bollards and/or cleats should be provided where existing and proposed moorings are located. These should be of robust cast iron design (fig 117). The use of these items for decorative purposes should be avoided as they are superfluous and may not be as securely fixed and therefore unfit for purpose.

**Electricity points** will be required for visiting craft at selected locations along the quaysides. These must be mounted above highest tide level and close to the edge of the quay. The electricity points will require secure and waterproof covering.

**Bollards** should be used sparingly, mainly at points on pathways to prevent vehicle incursion. They should also be used at vulnerable points on pavements for the same reason. The bollard featuring the Faversham crest should be used. (fig 118)

**Railings** Where existing galvanised steel tube railings are used (particularly on the north side of the Bridge), they should be replaced by cast iron posts painted black with steel railings as in fig 119.

**Posts** Timber posts (minimum section 150 x 150 mm) preferably of hardwood and suitably treated should be used in the 'greener' areas of the creekside. Posts would prevent incursion of vehicles onto grassed or other soft areas, such as creekside banks, where this is seen to be a problem. Posts should be set at 1.800 centres and be of approximately 600mm height (fig 120).

5.06  
**Code for Footpaths and Paved Areas**

All existing traditional paving materials (granite setts, kerbs, 'wheelers' etc and brick pavers) should be regarded as heritage assets and retained in situ. Where areas of traditional paving have been overlain by a coating of tarmacadam, the underlying traditional material should be exposed and retained.

Where areas are already paved in exposed aggregate concrete (e.g. areas of Standard Quay) and are

used by heavy vehicles, these should be retained, if they are not detrimental to the character of working quaysides.

The general principle underpinning this code is that the relatively formal, urban, established quayside areas should be paved with slabs, kerbs etc, and the informal, 'softer' areas should be paved less formally with a gravel finish with flush kerbs or porous, compacted aggregate (see 5.02 and 5.03).

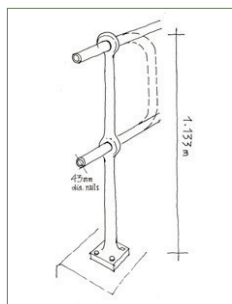


fig 119

**fig 119**  
Railings: cast iron posts and steel tubes appropriate for quaysides. This type is used by British Waterways.

**fig 120**  
Timber post to deter cars from parking or overrunning on grassed or other softer areas.

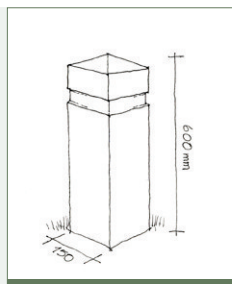


fig 120

Faversham Creek Streetscape Strategy

## 5.00 The Plan. continued

**Palette Of Materials For Formal Urban Areas.****(a) Natural stone paving slabs (York stone or Pennant stone)**

These should be laid with slabs of three widths and random lengths, 5-8mm joints, and minimum overlap of 75mm. Slabs to be laid in courses perpendicular to the kerb. They should be laid to crossfalls of approximately 1 in 40. Corners to be set out with radial cut slabs (fig 121).

**(b) Kerbs**

Where granite kerbing is available, or when abutting granite sett paths, this should be used. Otherwise 'conservation' kerbing shall be used. Kerbs will either be at 125mm above the carriageway surface or flush with carriageway, as appropriate.

**(c) Granite setts**

These should be used in quayside margins especially where heavy use is anticipated. They should also be used at 'crossover' points to sites. They may be considered appropriate in banding to reinforce speed reduction design.

In all cases setts should be approximately 200 x 100mm and of appropriate depth, bedding and jointing width to withstand the

loadings anticipated. Setts should be laid in stretcher bond and have secure edge detailing to prevent loosening.

**(d) Pavers**

Red brick (to semi engineering standard, minimum) with wirecut or similarly textured finish, laid in stretcher bond, compacted on a sand base. Pavers must be laid to a firm edging.

Pavers will be laid as narrow paths between buildings, as margins and in limited parking areas.

**(e) Exposed aggregate concrete**

Laid in bays, minimum of 2.5m and maximum of 3.0m wide, with tamped edge finish. This material to be used only as a continuation of concrete quaysides, which is subject to heavy vehicle usage and parking and where it is appropriate to the character of the area.

**(f) Surface dressed Macadam.**

Local aggregate rolled into a macadam (or resin base) to Kent County Council specification, for (1) light vehicle usage (access) or (2) cycle and footpaths.

**Palette Of Materials For Informal 'Softer' Areas****(g) Surface dressed macadam**

(see (f) above) will be appropriate in certain locations where heavier loading is anticipated, the edging details, (usually flush kerbing or flush setts), will complement the character of the area.

**(h) Compacted Gravel.**

For informal footpaths in landscape/soft settings. Treated timber board edging, contained by posts, flush with the path.

**(i) Ground reinforcement.**

Typically a light to medium strength matrix of recycled polyethylene or steel filled with gravel and seeding, appropriately edged. This provides a porous surface appropriate for light usage by vehicles for access and parking.

## 5.07

**Guidance for Creekside Development Sites**

A number of development opportunity sites front onto the creek which may be brought forward during the period covered by this strategy. It is essential that the requirements of this strategy are recognised and

incorporated not only in the planning, but the detailed design of proposals, in order to ensure

**(a)** the accessibility, continuity and legibility of the creekside.

**(b)** that potential connectivity between the creekside and the streets and footpath network of Faversham is achieved.

**(c)** that the frontages facing the creek are 'active' and do not consist of blank walls.

**(d)** that paved surfaces, street furniture, and, where appropriate, railings and lighting follow the guidance in this document, in order that the character of the creekside and its conservation area is preserved and enhanced

**(e)** that the scale, sense of enclosure and alignment of paths and frontages are appropriate to the character of the area.

Guidance should be taken forward into the Neighbourhood Plan to ensure that the principles, guidelines and projects of this strategy are implemented, in conjunction with the Faversham Joint Public Realm Working Group.

Faversham Creek Streetscape Strategy

## 5.00 The Plan. continued

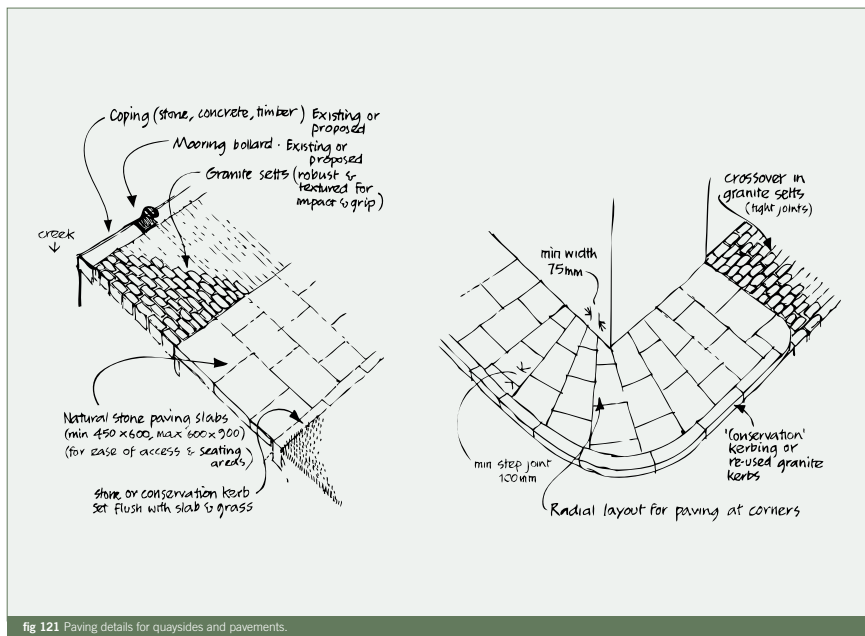


fig 121 Paving details for quaysides and pavements.

## 6.00 The Projects

## 6.01

## Introduction to Projects

The following projects present the main opportunities for intensive coordinated improvements around the creek. They are identified as being discrete interventions to achieve specific objectives in terms of the overall aims of the Strategy (section 4.00) and to address one or more of the negative issues identified in the Appraisal of the creekside public realm (section 3.00). The projects, each self contained, would contribute to the overall improvement of the creekside.

The projects are located on the Strategy map, (fig 122). Signage is shown on fig 112.

Whilst the projects are not ranked in any order of priority, it is stressed that the improvements to Town Quay and to Quay Lane would create a direct and attractive gateway to the creekside, whilst improving the setting of considerable heritage assets. The projects have been given an indicative costing (section 7.00) to guide allocation of resources.

In a number of cases, there are development opportunities on adjacent sites to the projects. In order that both the public realm projects and the proposed developments are designed to achieve environments of quality which will enhance local distinctiveness, it is strongly advised that schemes are coordinated through the Neighbourhood Plan.

## 6.00 The Projects. continued

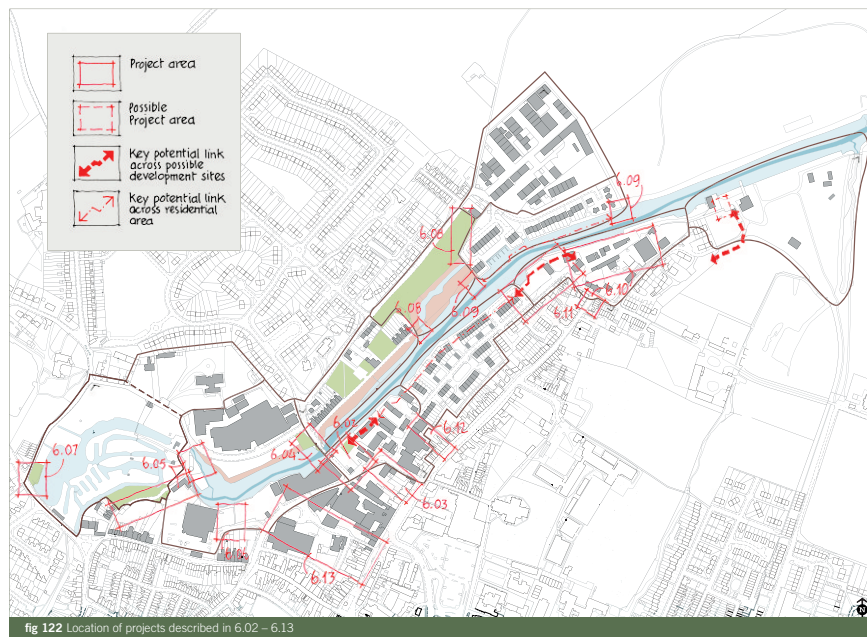


fig 122 Location of projects described in 6.02 – 6.13

## 6.00 The Projects. continued

## 6.02

**Town Quay and Green**

This area has been identified as being of considerable significance (section 2.03) and having a number of negative aspects which require improvement. It is an area which is also identified as both important and having considerable potential in the Faversham Streetscape Strategy (2005). The indicative plan (fig 123) shows the proposals.

**Objectives and specific guidance for the improvement of the area.**

**(a)** To create a direct visual and pedestrian link to the Quay along the sight line from Court Street.

A formal footpath of York Stone (detailed as in fig 121 and the Code 5.06) will lead from the junction of Quay Lane and Conduit Street to Town Quay, along the north eastern boundary of the site. Any future development fronting this boundary should directly abut the pavement, to ensure the intimate alley character of the townscape of the town centre. Proposed frontages should be 'active' i.e. should be enlivened by windows and doors. In the shorter term the existing fence on this boundary should be replaced by a 1.8m high

brick wall, appropriately detailed, (e.g. English garden wall bond, with brick on edge coping and piers as necessary; bricks to match the local traditions).

One or two lights, either wall mounted or standard will be required, and a footpath sign (preferably wall mounted, will be essential.

**(b)** To improve the setting of the heritage assets of the Listed Building (TS Hazard), the Edwardian building currently used as a boxing club and the stretch of traditional paving on the south west boundary of the site. This area to be paved in macadam surface dressed with gravel. Any drainage channels and edging required to protect the listed building, to be formed in clay pavers as specified in the code 5.06(d). The existing informal parking which surrounds the listed building at times should be reduced to the absolute minimum and organised in bays delineated by pavers.

The electricity sub station must be appropriately screened if it is impracticable and too costly to relocate it to a less intrusive location. A brick wall, between 1.8m and 2.1m high with full height timber access gates would be appropriate. The

detailing of the wall should be influenced by the materials and brick bond of the Edwardian building which it abuts.

The pavement on the Conduit Street frontage should be of York Stone as specified in 5.06(a). The difference in level between the pavement and the levels around the Listed Building should be detailed as steps.

**(c)** To ensure appropriate paving and detailing to Town Quay to facilitate potential connectivity to the adjacent quay walls.

A quayside margin of approximately 2.0 metres should be paved with granite setts, to provide a robust surface to withstand the usage associated with the mooring of vessels. Additionally a strip of approximately 1.5 metre on the landward side of the setts should be paved with York stone slabs to provide a smoother surface for walking.

Mooring bollards or cleats and mooring rings, should be provided. A suitably protected electricity point mounted above the level of highest predicted tides and a lifebelt, will also be required. At least one seat, to the specified 'Faversham' design should

also be included, as shown on the plan.

It is hoped that with the agreement of adjacent landowners, the Town Quay frontage may be extended. In particular, this possibility should initially be negotiated with Shepherd Neame, the owners of the car park between Bridge Road and Town Quay and its green. This would create continuity between two critically important features, the Bridge and Town Quay. A margin of approximately 5 to 7 metres would secure this connection to the landward side of the small building associated with the bridge, both of which are heritage assets. The surface would be of the same specification as that of Town Quay. A replacement screen wall, piers and railings would be necessary. This should be designed to create visual interest whilst providing appropriate security for the car park and should positively avoid the appearance of a 'dead' frontage.

However, it is desirable that the green is also extended, subject to agreement, to produce a coherent and convenient public open space. The indicative plan illustrates the extent of this area.

## 6.00 The Projects. continued

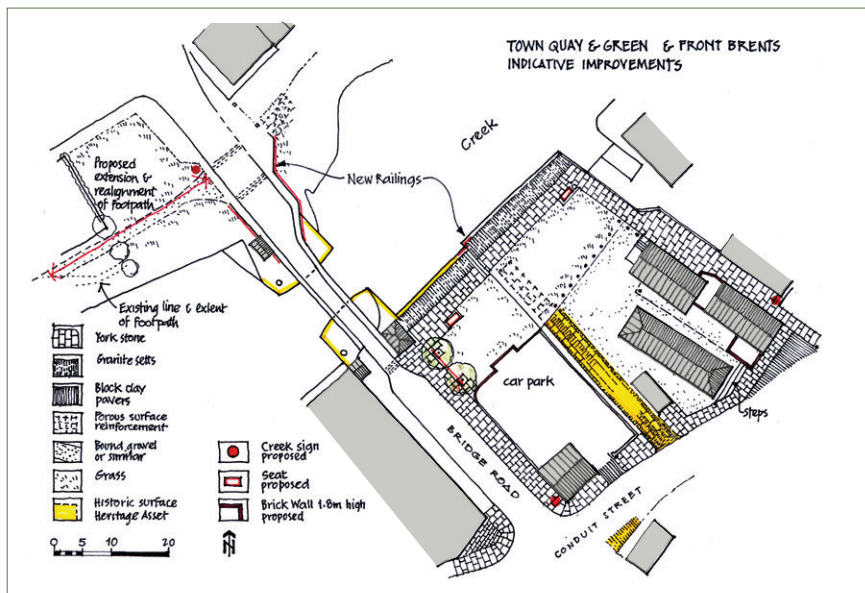


fig 123 Town Quay and Green showing public realm scheme and possible extension of quay and green towards Bridge Road (6.02). This map also shows the improvement scheme for the bridge and Front Brents area (6.04).

Faversham Creek Streetscape Strategy

## 6.00 The Projects. continued

The existing green would be retained, although the bollards and chains on the creekside may be considered redundant, given the existence of those on the landward side. It may be considered desirable to construct a semi porous reinforced surface (see code 5.06(ii)) between the stretch of traditional paving and the Quay, for the movement of equipment etc between moored vessels and the parked cars.

Future redevelopment of the north eastern side of Town Quay should be the subject of a neighbourhood Plan policy with the object of ensuring public access along the quayside, whilst retaining the possibility of using the slipway, if feasible.

## 6.03

**The 'Square' and Quay Lane**

This area is dominated by traffic, often constant and to a significant degree consisting of heavy goods vehicles. The sharp corner at the northern end of Quay Lane requires heavy articulated vehicles to cross the centreline of the road to complete the turn, and give way to each other approaching in opposite directions.

The Appraisal (3.01 (a)04 & (c)17) has identified the very narrow pavements (less than 1 metre wide) along Quay Lane as being both unattractive and inconvenient for increased pedestrian usage of this key link to the Town Quay area. The area of informal parking in front of the Swan and Harlequin pub, whilst necessary, could be accommodated in a way which enhances the area.

As the Appraisal (2.03) states, supported by the Faversham Streetscape Strategy, this area has great townscape potential which should be enhanced. The indicative plan (fig 124) shows the proposals.

Objectives and specific guidance for the improvement of the area

(a) Improve the pedestrian experience of Quay Lane and reduce

the impact of heavy traffic.

There is little space available to widen pavements or reduce the carriageway. However, existing vehicle speed and intrusiveness should be reduced through the design and layout of the 'Square' and the approaches from Quay Lane and Conduit Street, in order to give pedestrians a greater priority and safety. Placemaking solutions should be used to achieve this aim, as advocated in 'Manual for Streets' (2) and 'Streets for All' (South East volume). Within the context, two options should be considered, with the involvement of the highway authority.

**Option 1:** a 'tidal flow' scheme: has the advantage of slowing the approach speed to the square, and offering the potential of possible pavement widening where this is feasible and necessary. It could however, increase the speed of traffic where it has the right of way. A scheme should avoid traffic signals and road markings as these could be particularly at Court Street and Abbey Street.

**Option 2:** would be a scheme which would induce caution in drivers through the design and alignment of the carriageway surface, to reinforce

the sense of arrival in a square, where pedestrians have an equal right to be crossing. (see also (b) below.

The surface of Quay Lane pavement should be in York stone slabs. All granite kerbing should be retained.

At the lower end of Quay Lane and the corner into Conduit Street, the existing unsightly concrete plank wall could be replaced by a more appropriate solution. Initial discussions with Shepherd Neame indicate that the brewer would be willing to implement the building of a replacement wall. The indicative scheme shows the suggested realignment of the new wall, which could help to reinforce the sense of place of this important Square.

(b) *Improve the setting of the buildings on the north east of the area of 'The Square' and improve the pedestrian link to Town Quay.*

The indicative scheme illustrates the design principles incorporating this objective. The overriding principle is to adopt placemaking principles to (a) enhance the historic setting of the surrounding buildings and (b) calm traffic to give priority to pedestrians crossing in the area. Whilst the importance of the traffic route may

Faversham Creek Streetscape Strategy



## 6.00 The Projects. continued

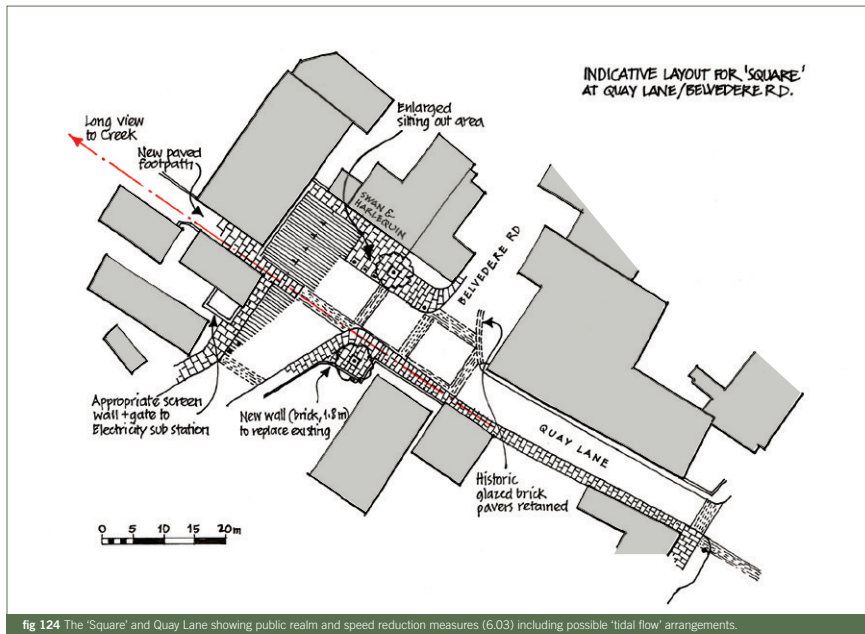


fig 124 The 'Square' and Quay Lane showing public realm and speed reduction measures (6.03) including possible 'tidal flow' arrangements.

Faversham Creek Streetscape Strategy

## 6.00 The Projects. continued

preclude a complete 'shared space' approach, the use of visual cues, changes in road alignment and encouraging people into the space should help to create a 'place' at this key location linking the town with the creek.

It is important that any proposals affecting the site on the west of 'The Square' recognise the important townscape role represented by any building fronting this space. A building here should be of a three storey scale and terminate the view from Court Street, but should not dominate the listed buildings in this area (see 5.07).

## 6.04

**Front Bents and link to path on north side of the Basin, including the Bridge.**

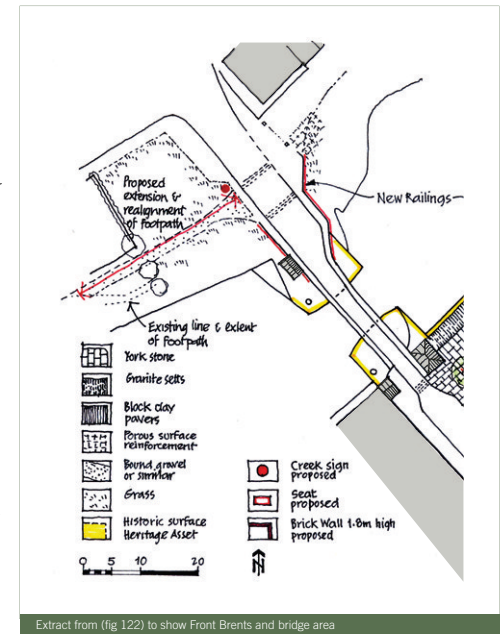
Objectives and indicative guidance for the improvement of the area.

(a) To repair and enhance the bridge and related structures in order to emphasise its importance to the creek, and to design these works in ways which would not preclude future projects for its full operation to be undertaken.

This project (fig 122 above) would be undertaken as a partnership with Peel Ports and Kent County Council, who own the structures. The small buildings on either side of the bridge would have doors and windows reinstated, and perhaps in one window recess an information board referring to the importance of the bridge, sluices etc. The gates either side of the bridge require repair and repainting. The 'kee-clamp' type railings should be replaced by railings of an appropriate 'waterways' style design (fig 119). The roadway deck requires repair or resurfacing. The bridge abutment platforms and associated machinery should be repaired.

(b) To facilitate the connectivity between Front Bents and the path on the north bank of the Basin.

The works would include:- construction of a path across the grassed area between the basin footpath and Bridge Road. The path should match the existing i.e. of compacted gravel with timber edging. Consideration should be given to integrating a courtesy crossing point with a 'give-way' point (for tidal flow traffic). A creek sign would be required at this point.



Extract from (fig 122) to show Front Bents and bridge area

Faversham Creek Streetscape Strategy

## 6.00 The Projects. continued

The existing plastic soil reinforcing matrix at the entrance to Front Brents may need to be replaced by more robust material and detailing and the existing bench supplemented by a 'serpent' pattern seat.

## 6.05

**Flood Lane to Brent Road**

Objectives and indicative guidance for the improvement of the area.

(a) The Lane between West Street and the Purifier building. To discourage the use of the area for parking, and the stabilisation of the surface whilst maintaining the bucolic and informal character of the area. The works should include: (fig 125)

- repair of the surface with compacted aggregate, to maintain a porous surface
- re alignment of the timber posts to limit the area for car parking
- erection of a creek sign at the junction with West Street.

(b) To improve the surface of the Lane between the west side of the Purifier building to Brent Road. The works could include:

- repair of the existing surface with compacted aggregate up to Ordnance Wharf.

- Compacted gravel or tarmacadam surface dressed with aggregate to minor access way standard, between Ordnance Wharf and Brent Road. This area to be designed as shared space, without kerbing. Bollards to be installed at either end of the Purifier building.
- the surface at Brent Road junction to be designed as a speed reduction area and to indicate a footpath in the roadway. The design should use materials rather than surface painting to indicate this objective.
- the two brick bridges and parapets should be inspected and repaired as required with matching materials.

## 6.06

**Improvements to the link between the south side of the Basin and North Lane.**

Objectives and indicative guidance for the improvement of the area.

(a) To improve the legibility and connectivity of this link whilst making it more attractive and safe to use.

The Appraisal has demonstrated (2.02) that the creekside quay path along the north side of Morrisons Store is underused and lacks apparent connectivity with the network overall, due to the

unattractive and potentially hazardous and indirect path running south to the approach road to Morrisons car park. It is essential that this link is substantially improved as an important component of achieving the aims of this strategy. The indicative illustration (fig 126) demonstrates how the negative aspects of this existing footpath link could be addressed and improved.

(1) Differences in level. The seven steps at the north end and three steps at the south should be replaced with by ramps at the gradient of 1 in 20. This would have a number of advantages:

(a) ramps signify the presence of a footpath, before it becomes apparent.  
(b) The ramps indicate that the footpath is accessible to all.

(2) Poor visibility. The acute angles and high, blank walls obscure the fact that a footpath exists. With the agreement of the landowners either side of the path (Morrisons and Shepherd Neames), slight realignments of the walls on the 'approaches' to the path could yield significant advantages in terms of legibility and accommodation of ramps.

(3) Poor legibility. The invisibility and apparent lack of connectivity would be addressed by the fixing of the proposed Creekside signs on the walls at either end of the path in locations which can be seen from a distance.

(4) Potential hazards for users, plus security issues for adjacent landowners: the existing lighting could be improved and extended, as shown, and the design of security fencing and anti intruder measures could be more attractive whilst maintaining its effectiveness, as shown.

## 6.00 The Projects. continued

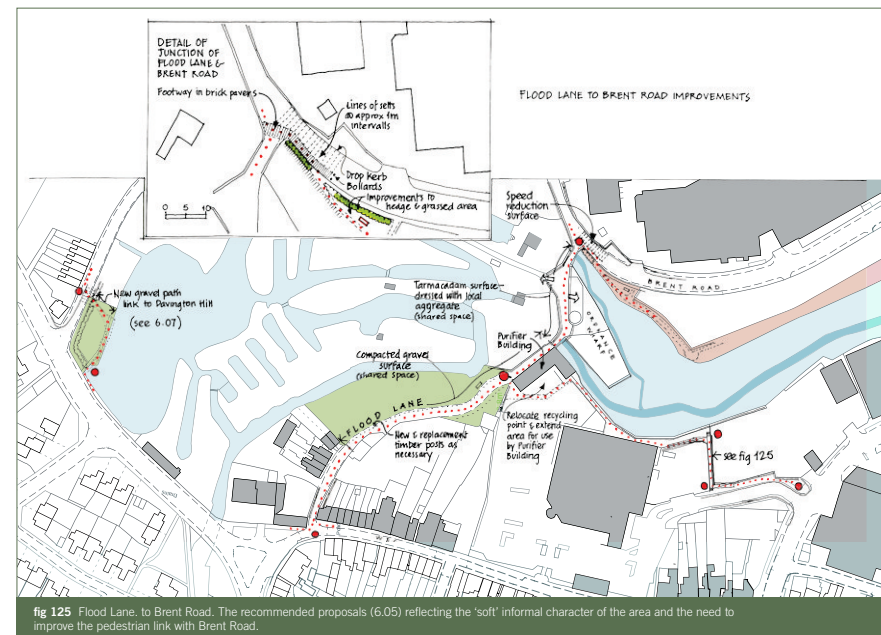


fig 125 Flood Lane, to Brent Road. The recommended proposals (6.05) reflecting the 'soft' informal character of the area and the need to improve the pedestrian link with Brent Road.

## 6.00 The Projects. continued

## SUGGESTED IMPROVEMENTS TO THE LINK BETWEEN THE SOUTH SIDE OF THE BASIN &amp; NORTH LANE.

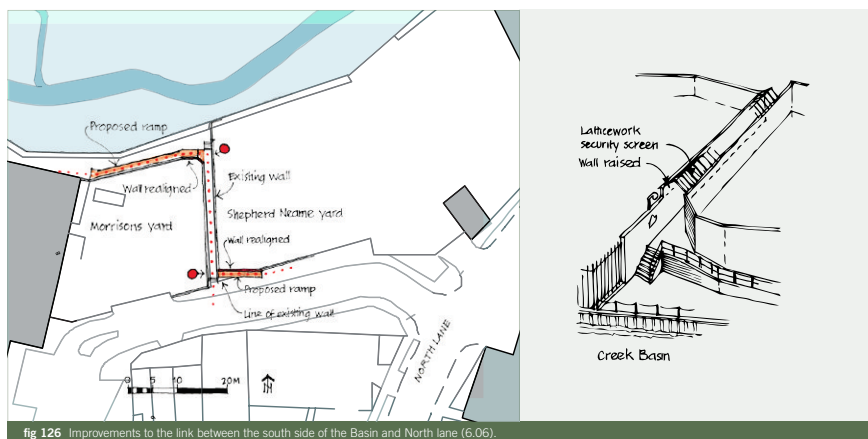


fig 126 Improvements to the link between the south side of the Basin and North lane. (6.06).

## 6.00 The Projects. continued

6.07

**Stonebridge Pond viewing area**  
Objectives and indicative guidance for the improvement of the area.

To create a link between Davington Hill and the small viewing area at the western end of Stonebridge Pond to improve the pedestrian experience at this location, and increase the visibility of the area

The works should include:

- Building a path to link with an opening formed in the northern boundary of the viewing area. This to be integrated with a courtesy crossing point across Davington Hill to the pavement in front of the cottages,
- Cleaning and repair of the seat and interpretation board.

6.08

**Crab Island to east end of Upper Brents**

Objectives and indicative guidance for the improvement of the area.

To improve and enhance the area around the culvert arch.

The works should include:

- Replacement of existing timber post and rail fence, when required, with similar of more robust design. Removal of disused interpretation board. If a board is considered necessary, this may be mounted on the wall of the Faversham Reach development.
- Replace the existing corrugated steel culvert by the construction of a bridge with arched openings and abutments constructed in brick, to be backfilled with earth and turf. Reinstate the footpath and erect appropriate timber posts and guard rails.
- Building a new path between the western corner of the Faversham Reach retaining wall to the north eastern end of Upper Brents. The path to be laid of compacted gravel with timber edging. Closing off the existing path between the north side of Faversham Reach development and the industrial premises, (see plan fig 127).

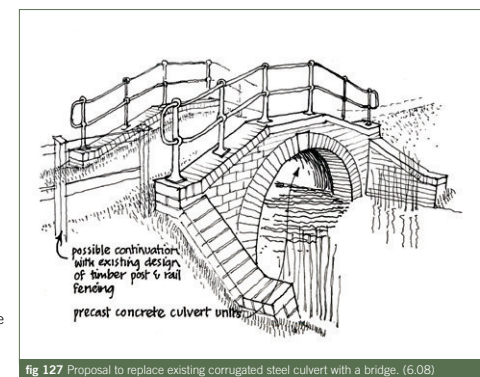


fig 127 Proposal to replace existing corrugated steel culvert with a bridge. (6.08)

## 6.00 The Projects. continued

6.09  
Faversham Reach and Connections.

Objectives and indicative guidance for the improvement of the area.

To explore the possibility of making a connection between the path on Crab Island and the Faversham Reach/Waterside Close quayside path, for public access.

- This project would require negotiation and agreement with the residents.
- Work could include a ramp between the level of Faversham Reach quayside and the path on Crab Island (approximately 1.5 metres) and another between the eastern end of Waterside Close and the dyke and footpath on the bank of the creek. Construction and detailing should be modest and 'waterfront' in character, in steel and timber (*fig 128*).
- Minor works including protective railings at key locations may be required on the quayside itself.

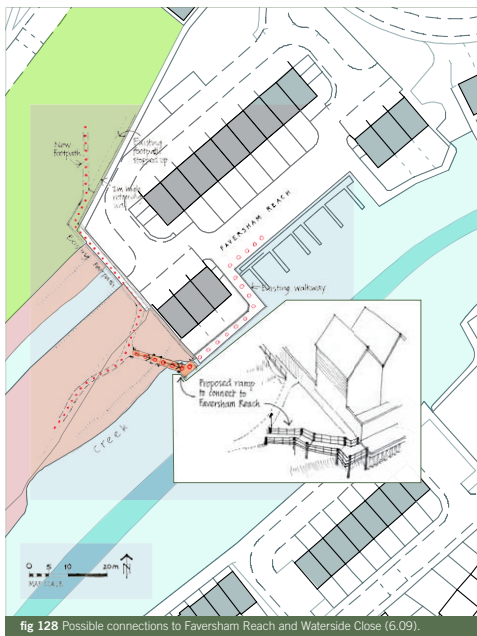


fig 128 Possible connections to Faversham Reach and Waterside Close (6.09).

Faversham Creek Streetscape Strategy



## 6.00 The Projects. continued

6.10  
Standard Quay

Objectives and indicative guidance for the improvement of the area.

To improve the signage for the area and the surfaces as necessary.

The works shown on the indicative plan (*fig 129*) illustrate the proposals which should include:

- 'Creek' signs installed at the north end of Abbey Street and at Standard Square.
- Resurfacing of the area on the south side of the weather boarded buildings as a shared surface, of surface dressing of aggregate on macadam. This to be informal in character but suitable for occasional parking and sitting out/display
- The passageways between the timber weather boarded buildings could be of brick pavers.
- The area could benefit from seating, using Faversham 'serpent' design.
- The area outside Standard House could be paved in York stone, extending no more than 2.5 metres. This would enhance the setting of the house.
- The existing concrete surface to the quayside is appropriate.



fig 129 Possible Improvements to Standard Quay (6.10).

Faversham Creek Streetscape Strategy

## 6.00 The Projects. continued

6.11  
The area on the southern end of Standard Quay and Standard Square

Objectives and indicative guidance for the improvement of the area.

To ensure the continuity of the links between Standard Quay and Belvedere Road.

The works should include:

- Retention of the rails embedded in the concrete surface.
- The repair and making good of the approach roadway to the bus depot
- Any development proposals for the area south of Standard Quay should ensure public access along the quayside/waterfront replacing or supplementing the path at the northern end of Belvedere Road. Reference should be made to the guidelines in section 5.07 in the planning and design of any proposals.
- The existing footpath at the north end of Belvedere Road to be resurfaced and signage installed at either end. This should be widened and undertaken notwithstanding any future developments

6.12  
Smack Alley

Objectives and indicative guidance for the improvement of the area.

This alley should be recognised as a key link to the creekside. Its surfacing and design should reflect this importance and its relationship to the historic characteristics of Abbey Street.

The works should include:

- Resurfacing of the alley with brick pavers laid with a crossfall to a gully (see the detail in the *Faversham Streetscape Strategy and fig 130*). The pavers to be set across Belvedere Road.
- A creek sign to be installed at the junction with Abbey Street, either as a hanging sign from a building or on a post.

6.13  
Partridge Lane

Although adjacent to the defined creekside area boundary, Partridge Lane is a major potential link between the town centre and routes to the creek. A direct line of sight between Court Street and the tower of Davington Church reinforces this link.

Whilst the eastern end of Partridge Lane has an attractive pedestrian character, the lower end of Partridge Lane and the junction with North Lane are hazardous and unattractive for pedestrians, as the area functions mainly as a service area for the brewery.

Objectives and indicative guidance for the improvement of the area.

- The potential of this lane as a main link between Market Place and the creek should be recognised in the improvement of its legibility.
- Pedestrian/vehicular conflicts should be mitigated by improvements to the pavements and crossing places.
- Opportunities for enhancement should be recognised in any proposals.

The scheme (fig 131) should include:

- improvement of the pavement surfaces.
- Creating a 'speed table' or similar speed reduction device at and just beyond the junction with Partridge Lane.
- Rationalising and enclosing the car park at the junction with North Lane.
- Reinstating the pavement on the North Lane frontage.

- 'tightening' the junction with Partridge Lane to control the speed of vehicles entering the Lane and improving the pavement.
- Installing creek signage at either end of Partridge Lane.
- Considering the installation of public art on the long blank wall of the brewery building on the north side of North Lane. This would aid speed reduction and provide visual focus and stimulation in this unattractive area.

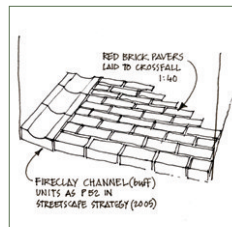


fig 130 Repaving scheme for Smack Alley; detail of surfacing design and materials (6.12).

## 6.00 The Projects. continued

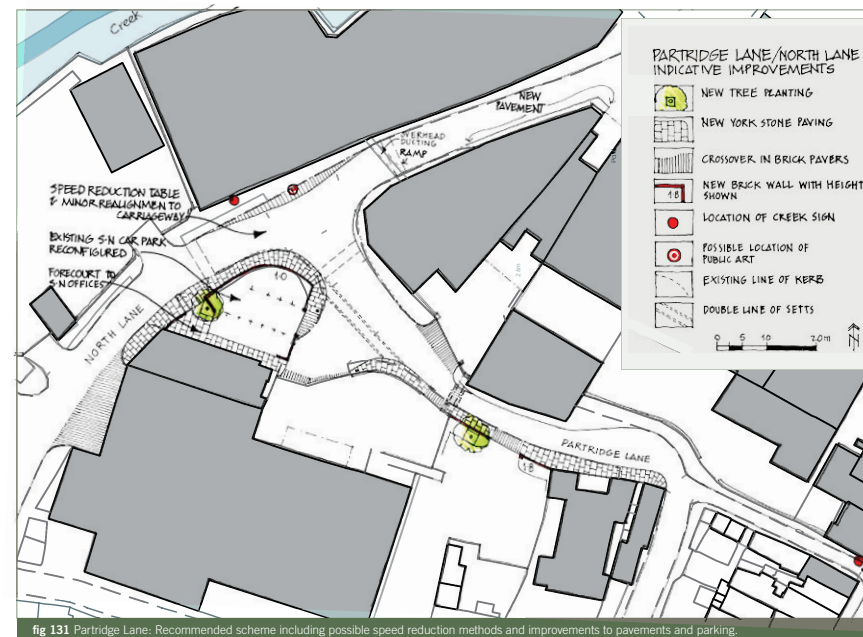


fig 131 Partridge Lane: Recommended scheme including possible speed reduction methods and improvements to pavements and parking.

## 7.00 Cost Summary

### 7.01

The projects identified in section 6.00 and specific items in The Plan, section 5.00, have been costed by Quantity Surveyors Mildred, Howells of Clifton, Bristol, based on the drawings, project descriptions and consultations with Context4D. These indicative costings were calculated in May 2012, at rates pertaining at that time.

Some items and projects will be undertaken by a partnership of various parties as they coordinate inputs by each. Thus, for example, a project may be funded by contributions from developers or landowners, the Town Council and the Highway Authority, amongst others.

General items such as the unit costs in the Code for example, are generic and can only be used as a guideline, as sub surface conditions will differ and the size of a particular contract will affect prices considerably.

### 7.02

#### Summary of Outline Budget Estimates

SECTION	PROJECT	COST (£)
6.04 / 8.02	Signage	4,000
6.02 / 8.03	Town Quay and Green	115,000
6.03 / 8.04	The 'square' and Quay Lane	184,000
6.04 / 8.05	Front Brents & link to path on North side of the Basin including the bridge	14,000
6.05 / 8.06	Flood Lane to Brent Road	90,000
6.06 / 8.07	Improvement to the link between the south side of the Basin & North Lane	30,000
6.07 / 8.08	Stonebridge Pond Viewing area	6,000
6.08 / 8.09	Crab Island to the east end of Upper Brents	149,000
6.09 / 8.10	Faversham Reach & Connections	90,000
6.10 / 8.11	Standard Quay	N/A
6.11 / 8.12	The area on the southern end of Standard Quay & Standard Square	N/A
6.12 / 8.13	Smack Alley	4,000
6.13 / 8.14	Partridge Lane	125,000

## 7.00 Cost Summary. continued

### Notes to the Outline Estimates

- All estimates are outline estimates based on the Context4D sketch details and are subject to checking when further details are prepared.
- The estimates are for the construction works only. They are based on current rates and exclude all fees and VAT.
- Each estimate includes an allowance for preliminaries, overheads and profit and a contingency of 10%
- Excavation, hardcore etc. included under all paved areas.

5. Assumed that the works will be competitively tendered to suitable small contractors.

6. Assumed that the works will be continuous with uninterrupted access.

7. Work around existing drainage and services, dimensions etc are excluded.

The following are outline estimated supply only rates. They are average rates for guide purposes only and actual rates will depend on type, location, situation, area etc.

ELEMENT COSTINGS	
Natural Yorkstone paving slabs	£55/m <sup>2</sup>
Conservation kerbs	£18/m
Granite setts	£45/ m <sup>2</sup>
Brick/block pavers	£28/ m <sup>2</sup>
Exposed aggregate concrete	£25/ m <sup>2</sup>
Surface dressed macadam for paths	£23/ m <sup>2</sup>
Compacted gravel including timber edgings	£20/ m <sup>2</sup>
Reinforced porous surfacing	£15/ m <sup>2</sup>

### 7.03

#### Approximate Measure

BOLLARDS, POSTS, RAILS, LITTER BINS AND LAMP POSTS <i>All fittings from the Broxap Range</i>	No	Per Item £	Total
Gillingham cast iron bollard ref BX 1542 RT (supply only £115, excluding town crest)	50	240.00	1200.00
Towpath cast iron bollard ref BX 1568.BP (supply only £39.90)	12	225.00	2,700.00
Timber bollard with chamfered top Ref BX 17 (supply only £72)	100	190.00	19,000.00
Bridgewater ductile iron 2 rail post system Ref BX 1572	/m	225.00	0.00
Derby Eros steel litter bin 80L capacity ref BX 2585 (supply only £495) assume no fixing required	10	550.00	5,500.00
Fittings below from the English Lamp Post Range.			
EL large swan neck lamp post 3.4m (supply only £749.99) excluding wiring, connections etc. which will depend on location etc.	00	1600.00	0.00

The above estimates exclude preliminaries, overheads and contingencies

## 8.00 Implementation

8.01

**In view of the general pressure on financial resources available to the public and private sector, it is envisaged that:**

(a) the timescale for the implementation and delivery of the projects and measures specified in this strategy may be between 5–10 years.

(b) That funding sources will be diverse and are likely to have their own criteria for implementation

(c) Some projects and programmes may be implemented as a component of planning agreements on Neighbourhood Plan policies as development proposals come forward.

(d) The community will set priorities for implementation through the Neighbourhood Plan.

(e) Local voluntary or charitable initiatives may propose to undertake some of the improvement, maintenance or repair projects.

(f) As many projects involve works within the adopted highway, they will be implemented in partnership with Kent County Council within the context of their programmes and priorities.

(g) Some projects may be eligible for Heritage Lottery Fund or Townscape Heritage Initiative support where important heritage assets are involved. The Faversham Joint Public Realm Steering Group will be the body responsible for coordinating the implementation of streetscape policies and projects.

8.02

### Creek Signage

#### Scheme

The scheme comprises the location and fixing of Creek nameplates as drawing (fig 111) at points identified on map (fig 112) following the location and design guidelines in section 5.04

#### Lead Agency

The scheme would be coordinated by the Faversham Joint Public Realm Working Group.

#### Cost (indicative)

The cost of supply and fix for each sign is estimated as £135. Approximately 30 signs would be required in total:- £4,050, (rounded to £4,000 on 7.02).

#### Delivery/Phasing

The first phase of 12 – 15 signs to be located at all gateways and where

routes are particularly illegible. This project could be implemented at an early stage following the approval and production of the signs. The project would have the advantages of having high visibility and completion in a short period. The funding source would be as the existing signage programme. There may be scope for sponsorship of individual or multiple signs, but no logo or identifying text should be added to the sign. Instead, a register of sponsors for this or any other project could be established.

8.03

### Town Quay and Green

#### Scheme

This is identified in section 6.02 of this Strategy.

#### Lead Agency

Faversham Joint Public Realm Working Group, in partnership with the conservation section, Swale Borough Council and adjacent landowners.

#### Cost (indicative)

£115,000 (this includes approx £23,000 for the extension of the green)

#### Delivery/Phasing

It is envisaged that this scheme and the adjacent Quay Lane and 'Square'

scheme will be given high priority as they were highlighted as being important in the Faversham Streetscape Strategy (2005). They would address a range of issues including securing a key access route to the Creek, protect and enhance major heritage assets, improve the sense of place and unlock the potential of adjacent sites.

8.04

### The 'Square' and Quay Lane

#### Scheme

This is identified in section 6.03 of this Strategy

#### Lead Agency

Faversham Joint Public Realm Working Group in partnership with Kent County Council Highways, Swale Borough Council and frontage owners.

#### Cost (indicative)

£184,000

#### Delivery/Phasing

It is envisaged that this scheme and the scheme for the adjacent Town Quay and Green, will be accorded a high priority as they were highlighted as important in the Faversham Streetscape Strategy (2005). This scheme would address a range of issues including securing a key

## 8.00 Implementation. continued

pedestrian access route to the Creek. Give the pedestrian greater prominence, tackle the negative impact of traffic, enhance the sense of place and unlock the potential of adjacent sites.

8.05

### Front Brents and link to path on north side of the basin including the Bridge

#### Scheme

This is identified in section 6.04 of this Strategy

#### Lead Agency

Faversham Joint Public Realm Working Group in partnership with Kent County Council Highways, Peel Ports and possibly with Faversham Creek Trust and other voluntary bodies.

#### Cost (indicative)

£14,000. Any future works to the Bridge to be estimated separately with Peel Ports and Kent County Council.

#### Delivery/Phasing

The link between Front Brents and the path to the north side of the Basin could be implemented within a short period and over a short timescale. The work related to the

bridge would require negotiation with the two major stakeholders above. It is possible that some elements of the improvement work could be undertaken by voluntary groups.

8.06

### Flood Lane to Brent Road

#### Scheme

This scheme is identified in section 6.05 of this strategy.

#### Lead Agency

Kent County Council Highways, and Faversham Joint Public Realm Working Group.

#### Cost (indicative)

£90,000

#### Delivery/Phasing

This project could be delivered in two phases:

- (a) Flood Lane to the entrance with Morrisons  
(b) The track on the north of the Purifier building to the junction with Brent Road. Phase (b) should be implemented in conjunction with any future development of Ordnance Wharf. However, it is recommended that if no development is approved within 3years, the work should be carried out to prevent serious deterioration of the surface.

8.07

### Improvement to the link between the south side of the Basin and North Lane.

#### Scheme

This is identified in section 6.06 of this strategy

#### Lead Agency

Morrisons and Shepherd Neame brewery in partnership with Faversham Joint Public Realm Working Group.

#### Cost (indicative)

£30,000

#### Delivery/Phasing

The scheme would be delivered following agreement between the adjacent landowners Morrisons and Shepherd Neame, regarding design, layout and contributions to the works. In view of the issues of personal safety and the revitalisation of the basin, this project should be recognised as having a relatively high priority.

8.08

### Stonebridge Pond viewing area.

#### Scheme

This scheme is identified in 6.07 of the strategy. (The scheme includes

the speed reduction table on Davington Hill).

#### Lead Agency

Swale Borough Council, Faversham Town Council, Faversham Joint Public Realm Working Group and Kent County Council.

#### Cost (indicative)

£6,000

#### Delivery/Phasing

Phase 1: the creation of the path to connect to the frontage on Davington Hill would be a small proportion of the total cost and could be delivered at an early stage.

Phase 2: the creation of the speed reduction table adjacent to the new connection of the path could be undertaken at the same time with similar work at the Brent Road/Flood Lane junction (see 8.06 above).

## 8.00 Implementation. continued

8.09

**Crab Island to the east end of Upper Brents****Scheme**

This scheme is identified in section 6.08 of this strategy.

**Lead Agency**

Swale Borough Council and Faversham Town Council/Faversham Joint Public Realm Working Group.

**Cost (indicative)**

£149,000

**Delivery/Phasing**

The closing off of the eastern end of the path and construction of a new path to Upper Brents should be a high priority. The reconstruction of the bridge replacing the culvert arch to be agreed.

8.10

**Faversham Reach and Connections****Scheme**

This scheme is identified in section 6.09 of this strategy.

**Lead Agency**

Kent County Council, (for Public Right of Way matters), in association with Faversham Joint Public Realm Working Group, in partnership with the residents of Faversham Reach and Waterside Close.

**Cost (indicative)**

£90,000

**Delivery and Phasing**

This scheme would rely on the outcome of negotiations and legal agreements with residents, and the identification of financial resources. It should be implemented as a single phase.

8.11

**Standard Quay****Scheme**

This scheme is identified in section 6.10 of this strategy.

**Lead Agency**

The owners of Standard Quay in partnership with Faversham Joint Public Realm Working Group, and Kent County Council Highways (for Public Right of Way matters and other public highway access).

**Cost (indicative)**

N/A

**Delivery and Phasing**

This scheme would be delivered in phases in conjunction with development proposals and possibly in part through planning agreement(s).

Faversham Creek Streetscape Strategy

8.12

**The area on the southern edge of Standard Quay and Standard Square.****Scheme**

This scheme is identified in section 6.11 of this strategy.

**Lead Agency**

The owners of the former oil depot and coach depot and as appropriate the owners of Standard Quay.

**Cost**

Not applicable. This would depend on the cost/design of any proposed development

**Delivery and Phasing**

This should be delivered through the conditions of any planning application, or in negotiation with the Faversham Joint Public Realm Working Group.

8.13

**Smack Alley****Scheme**

This scheme is identified in section 6.12 of this strategy.

**Lead Agency**

Faversham Town Council and the Faversham Joint Public Realm Working Group, with Kent County Council.

**Cost (indicative)**

£4,000 for surface; (excludes signage as that item is included in 8.02/6.01).

**Delivery and Phasing**

The scheme should be implemented at an early stage as it is a key access to the creek area, and would constitute an enhancement of the conservation area.

8.14

**Partridge Lane****Scheme**

This scheme is identified in section 6.13 of this strategy.

**Lead Agency**

Shepherd Neame and Kent County Council highways, in partnership with Faversham Joint Public Realm Working Group.

**Cost (indicative)**

£125,000

**Delivery and Phasing**

The scheme should be implemented at an early stage as it is an important potential access to the creekside area, and would improve the appearance of North Street and also contribute to pedestrian safety and convenience.

## 9.00 Appendix 1 Glossary

**Active frontage**

A building frontage at street level which enlivens the street or space which it fronts, by having windows, entrances and/or gardens facing the street. By contrast a dead frontage would be characterised by unrelieved blank walls facing the street.

**Heritage Asset**

"A building, monument, site, place, area or landscape identified as having a degree of significance meriting consideration in planning decisions, because of its heritage interest. Heritage asset includes designated heritage assets and assets identified by the local planning authority ..." National Planning Policy

**Framework 2012.**

Legible (and legibility)

A route which is easily perceived or understood; it is easy to find one's way.

**Neighbourhood Plan**

"A plan prepared by a Parish Council or Neighbourhood Forum for a particular neighbourhood area (made under the Planning and Compulsory Purchase Act 2004)". National Planning Policy Framework 2012.

**Placemaking**

The creation of places for people; involving the design of the public realm, determining the nature of pedestrian or vehicular access and connectivity, the consideration of the surrounding built form and the encouragement of mixed uses and active frontages.

**Public Realm**

A space which has unrestricted public access.

**Strategy**

A plan with stated aims and objectives, an overall framework for decisions and a programme of related actions stemming from it.

**Streetscape**

The 'landscape' of a street or similar route, i.e. the character of the street and its surrounding buildings or landscape. 'Street' in this context is taken as any path, quayside or other route as well as street. It can have an urban or rural character.

Faversham Creek Streetscape Strategy



## 9.00 Appendix 2

### COMMENTS ON THE EXHIBITION OF THE DRAFT STRATEGY, & RESPONSES BY THE CONSULTANTS

	COMMENT	RESPONSE
1. <b>Mary Kemp</b>	<ul style="list-style-type: none"> <li>Request for whole creek to be planned in a coordinated way.</li> <li>Access to creek for all.</li> </ul>	<ul style="list-style-type: none"> <li>Noted. This is a major objective of the strategy regarding streetscape coordination.</li> <li>Noted. It is a major objective of the strategy to encourage increased access to the creek.</li> </ul>
2. <b>Nova Coombs</b>	<ul style="list-style-type: none"> <li>More signage to creek from town.</li> <li>More public rights of way either side of creek.</li> <li>Dredging, repair of lock gates, tourist uses.</li> </ul>	<ul style="list-style-type: none"> <li>Noted. This is an objective of the strategy.</li> <li>Noted. This is an objective of the strategy.</li> <li>Noted. but these aspects are beyond the remit of the strategy.</li> </ul>
3. <b>Eric Glynn</b>	<ul style="list-style-type: none"> <li>Fully support.</li> <li>Desirability of having Plan strengthened by referendum.</li> </ul>	<ul style="list-style-type: none"> <li>Noted.</li> <li>Noted.</li> </ul>
4. <b>A. Taylor</b>	<ul style="list-style-type: none"> <li>This is excellent.</li> <li>Largely depends on support of frontage residents.</li> </ul>	<ul style="list-style-type: none"> <li>Noted.</li> <li>Noted. This consultation is part of the process.</li> </ul>
5. <b>Ken Brigden</b>	<ul style="list-style-type: none"> <li>Consider one way road system, Abbey St/Belvedere Road.</li> </ul>	<ul style="list-style-type: none"> <li>Noted. Abbey St is largely outside the remit of this study, but as it and Belvedere Road relate to Quay Lane, it will be considered.</li> </ul>
6. <b>Jim Richards</b>	<ul style="list-style-type: none"> <li>Strong support for ideas and plans.</li> <li>Important to recognise creekside character.</li> </ul>	<ul style="list-style-type: none"> <li>Noted.</li> <li>Noted. This is a major objective of the strategy.</li> </ul>
7. <b>Jim Richards</b>	<ul style="list-style-type: none"> <li>The area looking scruffy.</li> </ul>	<ul style="list-style-type: none"> <li>Noted. The strategy aims to encourage maintenance appropriate to the character of the area.</li> </ul>
8. <b>Robin Bowyer</b>	<ul style="list-style-type: none"> <li>Full support for enhancement.</li> </ul>	<ul style="list-style-type: none"> <li>Noted.</li> </ul>
9. <b>Helen Peters</b>	<ul style="list-style-type: none"> <li>Support for signage in principle &amp; design.</li> </ul>	<ul style="list-style-type: none"> <li>Noted.</li> </ul>
10. <b>No name</b>	<ul style="list-style-type: none"> <li>Pathway should be connected all round creek.</li> </ul>	<ul style="list-style-type: none"> <li>Noted. This is a general aim of the strategy</li> </ul>

## 9.00 Appendix 2. continued

	COMMENT	RESPONSE
11. <b>M Perkins</b>	<ul style="list-style-type: none"> <li>Flood Lane: don't make surface too smooth; slow speed essential as this is a play area.</li> <li>Provender Walk Path – should be reinstated:</li> <li>Ordnance Wharf – notes re design and use of proposals.</li> <li>Don't make proposals sterile.</li> </ul>	<ul style="list-style-type: none"> <li>Noted. This will be considered in the design.</li> <li>Noted. The strategy seeks to create links along the wharfs and quays.</li> <li>Noted. although this is largely beyond the remit of this strategy, however, the strategy does advocate design briefing for sensitive sites.</li> <li>Noted. The strategy is based on maintaining the character of the area.</li> </ul>
12. <b>Jane Knight</b>	<ul style="list-style-type: none"> <li>This is excellent.</li> <li>Largely depends on support of frontage residents.</li> </ul>	<ul style="list-style-type: none"> <li>Noted.</li> <li>Noted. This consultation is part of the process.</li> </ul>
13. <b>Janet Negus</b>	<ul style="list-style-type: none"> <li>Consider one way road system, Abbey St/Belvedere Road.</li> </ul>	<ul style="list-style-type: none"> <li>Noted. Abbey St is largely outside the remit of this study, but as it and Belvedere Road relate to Quay Lane, it will be considered.</li> </ul>
14. <b>No name</b>	<ul style="list-style-type: none"> <li>Strong support for ideas and plans.</li> <li>Important to recognise creekside character.</li> </ul>	<ul style="list-style-type: none"> <li>Noted.</li> <li>Noted. This is a major objective of the strategy</li> </ul>
15. <b>Debbie Lawther</b>	<ul style="list-style-type: none"> <li>General Support</li> <li>Request for continuous path round the creek.</li> <li>Restore traditional boat-related activities.</li> </ul>	<ul style="list-style-type: none"> <li>Noted.</li> <li>Noted. This is a major objective of the strategy.</li> <li>Noted. This is beyond the remit of the strategy.</li> </ul>
16. <b>Barbara Kelly</b>	<ul style="list-style-type: none"> <li>Retention of waterway identity.</li> <li>Traditional boat repair uses.</li> <li>Accessibility is all, therefore improvement of access and footways important.</li> <li>Signage needed.</li> <li>Don't want 'domesticated' character</li> </ul>	<ul style="list-style-type: none"> <li>Noted. This is one of the objectives of the strategy.</li> <li>Noted. This is one of the factors contributing to local character, although control of land use is beyond the remit of the strategy.</li> <li>Noted. This is one of the objectives of the strategy.</li> <li>Noted. This is one of the objectives of the strategy.</li> <li>Noted.</li> </ul>
17. <b>Mrs P Brice</b>	<ul style="list-style-type: none"> <li>Support all aspects being considered.</li> <li>Wants to see more water related activity.</li> <li>Supports more residents' consultation.</li> <li>The town must be attractive as a showcase.</li> </ul>	<ul style="list-style-type: none"> <li>Noted.</li> <li>Noted. although this is beyond the remit of the strategy.</li> <li>Noted.</li> <li>Noted. It is an objective of the strategy to improve and maintain creekside in ways which are appropriate to its character,</li> </ul>
18. <b>Chris McCombe</b>	<ul style="list-style-type: none"> <li>Poor surfaces on Front Brents</li> </ul>	<ul style="list-style-type: none"> <li>Noted. The improvement of eroded surfaces is an objective of the strategy.</li> </ul>

## 9.00 Appendix 2

	COMMENT	RESPONSE
19. <b>Patricia Hall</b>	<ul style="list-style-type: none"> <li>• Welcomes signage plans and improved paths.</li> <li>• Concern over traffic along Abbey Street.</li> <li>• Uses and temporary uses.</li> </ul>	<ul style="list-style-type: none"> <li>• Noted.</li> <li>• Noted. although this area is outside the remit of the Strategy area.</li> <li>• Noted. although this is beyond the remit of the strategy.</li> </ul>
20. <b>Sue Davidson</b>	<ul style="list-style-type: none"> <li>• General support but may need fundraising.</li> <li>• General encouragement to people of the area.</li> <li>• Need for constant upkeep.</li> </ul>	<ul style="list-style-type: none"> <li>• Noted.</li> <li>• Noted.</li> <li>• Noted. This is a key objective of the strategy.</li> </ul>
21. <b>Christopher Wright</b>	<ul style="list-style-type: none"> <li>• Quay Lane: preference for shared surface over tidal flow solution.</li> <li>• Flood Lane: preference for permeable surface.</li> </ul>	<ul style="list-style-type: none"> <li>• Noted. These views will be taken into consideration.</li> <li>• Noted. These views will be taken into consideration.</li> </ul>
22. <b>G Turner</b>	<ul style="list-style-type: none"> <li>• Supports footpath proposals for Faversham Reach.</li> <li>• Supports all proposals, including surface of parts of Saxon Shore Way</li> </ul>	<ul style="list-style-type: none"> <li>• Noted.</li> <li>• Noted.</li> </ul>
23. <b>Derek Hitcham</b>	<ul style="list-style-type: none"> <li>• Supports better signage and paths and circular walk.</li> <li>• Advocates area being a maritime area and related uses.</li> </ul>	<ul style="list-style-type: none"> <li>• Noted.</li> <li>• Noted. The strategy aims to reflect the maritime character in the streetscape.</li> </ul>

## 9.00 Appendix 2. continued

FAVERSHAM CREEK TRUST RESPONSE TO STREETSCAPE STRATEGY PROPOSAL. 30TH MARCH 2012.

## GENERAL OBSERVATIONS:

1. A lot of time and money has been spent on the presentation and we welcome the opportunity to comment, but the proposed alterations appear superficial, random and without a proper focus. The primary concern of the Faversham Streetscape as it relates to the Creek should be the reinvigoration of the navigation. Everything else should flow from that: access, function, sustainability, safety, community, appearance and integration.
2. There is a once-only opportunity here for serious, long-term economic benefit for the town, and not just a matter of paving stones and picnic areas.
3. People in Faversham understand that the commercial revival of the Creek depends on the proper operation of the sluices, lock-gates and bridge. Once these are put back into working order, a huge slew of benefits will generate: the Basin will act as a cistern to sluice the mud away on the tides, more vessels will have more access to quays along the Creeks' urban length, tourism will expand, jobs will be created.
4. The Creekside Streetscape Strategy should align itself with and openly support this long-term economic programme: restore the working of the sluices, lock-gates and bridge. All the other changes and improvements should flow from that central act, or be judged against it. So, for instance, if a creek-side path will help or enhance the operation of the bridge, good. If it causes an obstruction for the working of the bridge, then don't go ahead with that bit of path.
5. The Strategy lacks any sense of perspective from the point of view of a mariner. Where are the moorings? Where are the water and electric points? Where are fresh water points? Where is the Harbour Master? Where will maritime events be held?

## SPECIFICS - THE BRIDGE COMPLEX

6. The lock gates must be made to open. The sluicing should be done on each high tide. The bridge should be swung.

7. This bridge is believed to have historic merit in its functional hydraulics. It may need repairs to strengthen part of its undercarriage, but our engineering advice is that this could be done as a straightforward welding job for less than £50,000.

8. The present 'deck' and railings of the bridge are modern (1970s?) and are unattractive. They are also dangerous for pedestrians and cyclists, as the footpaths are so narrow and the carriageway is worn and uneven. The handrails are boring and ugly. There is a strong case to be made for replacing at least part of this upper structure with something more attractive.

9. Alternatively commission an entirely new bridge, perhaps with a different lifting mechanism. There are many attractive examples in the country: St Katherine's Dock, Salford Quays, etc. We urge the council to be ambitious in its aspirations here, in terms of quality of design and operation. Look to the best examples.

10. In any event, the pedestrian experience of the bridge must be upgraded. Pedestrians and cyclists should have priority over vehicles. The whole appearance of the bridge – handrails, path surfaces, paintwork, signage, etc should be improved. Barriers will be needed to stop traffic when the bridge is opened for vessels to pass into and out of the Basin, and these should be designed and installed with a high aesthetic value, again with pedestrian priority and a sense of drama and occasion.

11. The opening of the bridge for navigation purposes presents an unusual and interesting opportunity for tourism. Any individual opening event will not last more than 10 minutes, so will not present an insuperable problem for road traffic especially if it is timetabled and advertised, and in any case can only happen at high tide – two small time-windows in each 24 hours. However, space must be made for tourists and others who want to watch and photograph this happening.

12. The lockgates are the responsibility of Medway Ports. We see an opportunity here for the Council to work with Medway Ports and others to establish a Harbour-Master's office on the Creekside. The Harbour Master would be the person who physically oversees the working of the lockgates

## 9.00 Appendix 2. continued

and sluices. This would therefore be a practical role but also ceremonial, with tourism opportunities. The Harbour Master's job could be funded in co-operation with others, such as the Faversham Creek Trust, boat-owners, etc.

**13.** The historic nature of the bridge and sluicing complex, its connection to the gunpowder industry and comparison with similar sluicing arrangements here and abroad should all be signed in some way to explain to visitors and local residents alike what is going on.

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#### THE BASIN

**14.** It is remarkable how quickly and efficiently one small boat has recently cleared a huge amount of mud from the gutway. This has allayed fears about the scale of the challenge of dredging, and the cost. Clearly this work must be continued, not least in the Basin where it is of most value for ongoing sluicing work.

**15.** Once the accumulations of mud have been removed from the Basin, it can become a regular berth for vessels of all kinds. We do not exclude commercial shipping from this list, especially for the future. Safeguarding this opportunity must be a high priority for the plan.

**16.** The wharf hidden behind mud banks in front of BMM Weston should be revealed and restored. We are thinking of it as Cleaver's Wharf.

**17.** Access to Cleaver's Wharf from road and on foot should have maritime priority. For instance, real mooring facilities are needed, and points for water, electricity, sewerage, etc.

**18.** The man-made nature of the present Basin and Creek (rough-stuff trade, Powder Monkey Bay, Telford history etc) are all important elements and should be signed at appropriate points along the proposed Creekside path.

**19.** There is space in the Basin for a new bridge - were a small island to be created as a dot at the end of the Ordnance Wharf 'exclamation' mark'. This could create a focus for visitors, a new destination, something interesting.

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#### FLOOD LANE

**20.** The rural atmosphere in Flood Lane is not accidental – it is essential to the gunpowder history of this area, where dangerous materials were moved by boat, and road surfaces remained un-metalled to prevent sparks. Process-houses were small, wooden and separated by trees to help minimise blast damage in the event of an accident. We strongly believe that the road surface in Flood Lane should not be metalled in any way. In fact the present gravelly surface should be carefully retained. It may be muddy for part of the time, but that is a small price to pay for a very valuable area of living history. Rus in urbs.

**21.** The gunpowder history of the area of Stonebridge Pond, Flood Lane, Ordnance Wharf and the Stonebridge Allotment area and walls should be signed and explained, as part of the walking experience.

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#### FOOTPATHS, PATHWAYS, SLIPWAYS

**22.** In general, Faversham's extraordinary and remarkable network of alleys, paths, passage-ways, footways and walks is totally un-recognised and could so easily be developed and promoted for tourists and local people alike. This is true around the Creek area but also in the medieval urban centre and the Victorian districts, where the brick walls create a beautiful backdrop.

**23.** Many urban pathways have been closed off for private purposes in recent years, including some leading to the Creek (eg from Abbey Street) and efforts could and should be made to get them opened again. Private owners may be asked to open them for special occasions or for a certain number of days per year, for instance.

**24.** The Creekside path has several undignified blockages which your proposals seek to open up, which we think is a good idea.

**25.** We think there should be a direct 'emotional' connection between the Creek and the paths for as long a distance as possible – so that people might walk along keeping pace with a moving boat, or race the sluice-flood on bikes.

## 9.00 Appendix 2. continued

**26.** Some emphasis must be given to the need for slipways, for boats of different sizes. Such slipways will need space around them for boat trailers. Slips will be needed both above and below the bridge, and further downstream.

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#### SHARED SPACE, TRAFFIC MANAGEMENT

**27.** In the sensitive and highly specialised areas near the water, we think shared space would work well, that is, with pedestrian and navigational priority. This is true for the bridge area as well as any wharves, quays, access roads etc.

**28.** Traffic management should also bear in mind that Faversham has only one main road artery. A hold-up at the bottom of Quay Lane can affect the Post Office area.

**29.** Top of Quay Lane. We do not think traffic lights are needed but road layout and sightlines for drivers here are dangerous, especially leaving Court Street or Abbey Street, and HGVs have tremendous difficulty turning.

**30.** Bottom of Quay Lane It could be that shared space principles could apply at here, with a 'piazza' space and visual access to the Creek, as per UI report.

**31.** The road layout and sightlines on the Brents side of the bridge are also hazardous and should be upgraded and improved for pedestrians and cyclists.

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#### SIGNAGE

**32.** There is at present almost no mention of the Creek at all in the town centre, or at the railway station, or the bus stops. An organised scheme of signage and maps could be installed immediately, indicating various interesting points along the water – Stonebridge Pond, the Basin, the Bridge, the Town Quay and the Town Green, Crab Island, Standard Quay, Iron Wharf and the Coastal Path.

**33.** The UI report exposed the lack of exciting vistas onto the Creek, but we think these would be hugely enhanced if there were interesting vessels to see on the water or mud.

**34.** There are only a few hours a day when there is any water in the Creek. Mud is the usual – and under-valued – surface. Signs should also explain this, and help people understand the strange, sculptural and natural beauty and ecological value of the mud.

**35.** Resident in Faversham at the moment are two world-famous creators: Chris Daniel who is the leading expert on sundials and navigation instruments, and Henry Dagg who makes sound sculptures. They could each be commissioned to design or make installations for the marketplace – something like a tide-clock, for instance. We should be using and celebrating local talent and knowledge for the benefit of the whole community.

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#### SUMMARY

**36.** The Streetscape exhibition and consultation was disappointing in its lack of any focus or dynamic drive. It did not in fact feel 'Strategic'. On the whole we do not disagree with the specifics but we feel very strongly there should be an overall motivator – ie the reinvigoration of the Creek navigation, from which all other improvements would flow.

**37.** Recent dredging activity by just two vessels – one entirely privately owned and paid for - has created a dramatic improvement in the capacity of the Creek to accept larger vessels, which is timely for the Queen's Jubilee. Instead of more and more reports, please can we have more money spent on REAL improvements like this?

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#### FCT 30/3/2012

**9.00 Appendix 2. continued**

CONSULTANTS RESPONSE TO THE FAVERSHAM CREEK TRUST PAPER. (NUMBERS CORRESPOND TO PARAGRAPHS IN FCT PAPER)

**1.** The proposed alterations relate to (a) the general aims and objectives of the strategy and (b) to a systematic analysis of locations where issues have been identified which it is felt require attention.

The primary concern of the strategy is the streetscape, i.e. the areas of mainly pedestrian and vehicular access, not the reinvigoration of the navigation, which is undoubtedly important.

**2.** Noted. The strategy is not merely an exercise in cosmetic treatment, but addresses matters of safety, accessibility, connectivity, 'legibility' and the enhancement of the character of the creek.

**3.** Noted. However, this is not within the remit of the strategy.

**4.** Noted. See note 3 above. It is acknowledged that any paths or access improvements should not hinder to working of the bridge.

**5.** Noted. The moorings are the existing ones. There is guidance in the strategy regarding electricity points, mooring bollards and cleats etc.

**6.** Noted. See note 3 above.

**7.** Noted. See note 3 above.

**8.** Noted. Agree: replacement of railings and some surfaces is a specific recommendation of the strategy.

**9.** Noted. See note 3 above.

**10.** Noted. Agree: the sensitive improvement of the bridge area is a specific recommendation of the strategy.

**11.** Noted. See note 3 above.

**12.** Noted. See note 3 above.

**13.** Noted. See note 3 above.

**14.** Noted. See note 3 above.

**15.** Noted. The advocacy of a particular use is beyond the remit of this streetscape strategy, although waterside uses are an important component of the character of the creekside area.

**16.** Noted. See note 15 above.

**17.** Noted. See note 15 above.

**18.** Noted. Improved interpretation is an objective of the strategy.

**19.** Noted. This would appear to be a very expensive proposal which could impede future access by vessels, intrude on the wide views across the basin and double up on existing north – south connections via Flood Lane/ Ordnance Wharf or the Bridge.

**20.** Noted. A permeable, informal surface treatment will be proposed.

**21.** Noted. See note 18 above.

**22.** Noted. The Town Centre Streetscape Strategy (2005) recognises this, as does the creekside strategy.

**23.** Noted. The strategy encourages the creation and reinstatement of links to the creek. It will include more emphasis on securing agreement of owners.

**24.** Noted

**25.** Noted. An interesting idea.

**26.** Noted. This will be investigated.

**27.** Noted. This is an objective of the strategy

**9.00 Appendix 2. continued**

**28.** Noted. The key importance of Quay Lane is acknowledged, and appropriate solutions are being considered.

**29.** Noted.

**30.** Noted: agree, this is being investigated.

**31.** Noted. See note 30 above.

**32.** Noted: this is a major objective of the strategy.

**33.** Noted: agree

**34.** Noted: agree

**35.** Noted: agree. This is an interesting idea, although the locations suggested are outside the remit of the creekside Strategy. The principle of encouraging public art by local artists will be advocated for specific locations.

**36.** Noted. The exhibition could only focus on some specific projects, due to the space available and the interim nature of the strategy. However, it did include objectives of the strategy.

**37.** Noted. See note 3 above.

## 9.00 Appendix 2. continued

COMMENTS FORMS RECEIVED BY SWALE BC BY 28TH MARCH 2012 AND FORWARDED TO THE CONSULTANTS.

	COMMENT	RESPONSE
24. <b>Peter G Ward</b>	<ul style="list-style-type: none"> <li>Small exhibition space made the access and study of the material difficult.</li> <li>Material well produced.</li> </ul>	<ul style="list-style-type: none"> <li>Noted. Limited by practicalities; it was considered that a prominent location at a busy time was desirable.</li> <li>Noted.</li> </ul>
25. <b>Mrs F A Ward</b>	<ul style="list-style-type: none"> <li>Confused, difficult to comprehend in the confined exhibition space.</li> </ul>	<ul style="list-style-type: none"> <li>Noted. Limited by practicalities; it was considered that a prominent location at a busy time was desirable. The exhibit was structured by the analysis being on one side and the proposals on the other.</li> </ul>
26. <b>Janice Ely</b>	<ul style="list-style-type: none"> <li>Crowded</li> <li>Streetscape in layman's term.</li> <li>Library exhibit unstaffed.</li> <li>Signs should be consistent with the established pattern. Do not like plastic proposal.</li> </ul>	<ul style="list-style-type: none"> <li>Noted. The consultant was present to assist in explanation.</li> <li>Noted. The term was defined on a panel, but a glossary will be included in the strategy.</li> <li>Noted. It is Swale BC practice for one day to be allocated for staffing of a public consultation exhibit.</li> <li>Noted. The design options for signage are being considered. Plastic is not being proposed.</li> </ul>
27. <b>Roger Ely</b>	<ul style="list-style-type: none"> <li>Jargon</li> <li>Small tent, little information.</li> <li>Spend the money on sluices etc.</li> </ul>	<ul style="list-style-type: none"> <li>Noted. The terms quoted are mainly titles of documents. However a glossary will be provided in the final document.</li> <li>Noted. It was considered that the location of a stall at the Market Place on market day was important, to attract as many people as possible.</li> <li>Noted. This is beyond the remit of the streetscape strategy.</li> </ul>
28. <b>Robin Grimble</b>	<ul style="list-style-type: none"> <li>Had difficulty in being able to see and study the exhibits.</li> </ul>	<ul style="list-style-type: none"> <li>Noted. The exhibit stall was the only space available. Other comments arise from difficulty in being able to see the material.</li> </ul>
29. <b>Griselda Mussett</b>	<ul style="list-style-type: none"> <li>Comments are identical to those in FCT comments.</li> </ul>	Please see response to these.
30. <b>Hilary Morley</b>	<ul style="list-style-type: none"> <li>Approve of reference to 'safe' in list of objectives, but not at expense of vessels.</li> <li>Hazardous routes comments.</li> </ul>	<ul style="list-style-type: none"> <li>Noted.</li> <li>Noted.</li> </ul>
31. <b>Brian Taylor</b>	<ul style="list-style-type: none"> <li>Suggestions in strategy seem reasonable.</li> <li>(1) Quay Lane. Comments on pedestrian safety.</li> <li>(2) Creek Bridge speed hazard.</li> </ul>	<ul style="list-style-type: none"> <li>Noted.</li> <li>Noted. This will be further considered in final draft.</li> <li>Noted. This will be further considered in final draft.</li> </ul>

## 9.00 Appendix 2. continued

	COMMENT	RESPONSE
32. <b>Marika Sherwood</b>	<ul style="list-style-type: none"> <li>Response to Neighbourhood Plan re the creek</li> <li>(a) Repair yard for boats.</li> <li>(b) The creek should be dredged.</li> <li>(c) Old buildings should be restored</li> </ul>	<ul style="list-style-type: none"> <li>Noted. Although this is beyond the remit of the streetscape strategy.</li> <li>Noted. Although this is beyond the remit of the streetscape strategy. One of its main objectives is to protect and enhance the setting of historic buildings by improving the surfaces around them.</li> </ul>
32. <b>Sarah Deeson</b>	<ul style="list-style-type: none"> <li>All ideas sound.</li> <li>Continuous, coherent access to creekside would be invaluable</li> </ul>	<ul style="list-style-type: none"> <li>Noted.</li> <li>Noted. This is a major objective of the strategy.</li> </ul>

## 9.00 Appendix 2. continued

EMAIL RECEIVED BY SWALE BOROUGH COUNCIL 12TH MARCH 2012

	COMMENT	RESPONSE
24. Hilary Whelan & Chris Berry	<p><b>1. The Process</b></p> <ul style="list-style-type: none"> <li>The process seems convoluted and protracted &amp; the proposals focus on footpaths, traffic calming and signs. How does this help keep buildings in proportion and encourage development which supports employment or amenity?</li> </ul>	<ul style="list-style-type: none"> <li>Noted. The purpose of the streetscape strategy is to provide a clear framework of objectives to aid decisionmaking, within the context of the Neighbourhood Plan. The remit of the strategy is the streetscape, not to determine the impact of development proposals or their uses.</li> </ul>
	<p><b>2. The Proposals</b></p> <ul style="list-style-type: none"> <li>Difficult to study the proposals and not well presented.</li> </ul> <p>Like: The proposals to link up the waterside path on both sides of the Creek and The Basin; tidying up and providing better signage and information.</p> <p>Don't like: Design of signs as proposed at the exhibition.</p>	<ul style="list-style-type: none"> <li>Noted. The limitations on space for the exhibition meant that the material had to be reduced. The proposals were at an interim stage. However it is accepted that some visitors found the exhibit confusing.</li> <li>Noted</li> <li>Noted. Whilst there was a wide divergence of opinion regarding the proposed sign design, the final proposals have opted for a variant on the existing street signs.</li> </ul>
	<p><b>3. Omissions</b></p> <p>3.1 The Bridge Walkways are narrow and crossing the bridge is hazardous. Suggested remedies: single wide walkway, non-slip surface, safe crossing points, speed limit.</p> <p>3.2 Cycling The existing provision of cycling routes is unattractive and at places hazardous for cyclists.</p> <p>3.3 The Basin The smell of the mud at low tide has not been considered. Other senses than the visual should be taken into account.</p> <p>3.4 Facilities and Services Should consider litter bins, upgrading street cleaning and there are no public toilets in the area.</p>	<ul style="list-style-type: none"> <li>Noted. These are helpful suggestions. Due to the ownership of the bridge and the large costs involved in its replacement or conversion, these matters would have to be negotiated with the owners. The redesign of the bridge has to be considered in conjunction with decisions regarding the sluice arrangements. Within these limits the final report will suggest traffic calming measures and improvements to the surface of the bridge.</li> <li>Noted. The proposals for improvement to surfaces, improved signage, improvements to linking up unconnected paths and speed reduction measures should help cyclists as well as pedestrians.</li> <li>Noted. This has not been a factor raised at any point in the preparation of the strategy. However any proposals for reduction in mud levels should reduce the problem. The key senses of place, convenience and personal safety have been central to the design proposals.</li> <li>Noted. The proposals include seating and litter bins. The improvement of surfaces and the closure and re routing poorly laid out parts of the footpath network should assist cleaning. The lack of public toilets is a valid issue which is related to but not within the remit of the strategy.</li> </ul>

## 9.00 Appendix 2. continued

	COMMENT	RESPONSE
	<p><b>4. Where's the Wow factor?</b></p> <p>Will the proposals be attractive enough to help in applications for funding? Is there anything imagination grabbing? Why no proposal for a new crossing point? This could attract funding</p>	<ul style="list-style-type: none"> <li>Noted. A strategy which creates a joined up network of pedestrian friendly routes, and which in so doing creates and improves the places and settings along the creek is important and as yet unusual. The aim is to achieve design solutions which relate to and derive from the character of Faversham and its creek. A new pedestrian crossing point could be appropriate (depending upon the plans for the bridge) and perhaps could be considered as development opportunities come forward. Nothing in this strategy precludes this possibility, but improvement of the existing pedestrian environment is the main purpose of this strategy.</li> </ul>

